



Statutory Planning Committee

Notice is hereby given that a meeting of the
Statutory Planning Committee will be held on:

Tuesday 25 March 2014
9.00 am

Level 2, Room 2.40
One40 William Street
Perth



Tim Hillyard
WAPC Secretary

Statutory Planning Committee

Membership:

Member	Representation in accordance with <i>Planning and Development Act 2005</i>	Term of office ends
Mr Eric LUMSDEN	Chairman, WAPC Schedule 2 clause 4(2)(a)	04/11/2016
Mr Duncan ORD	A/g Director General, Department of Planning Schedule 2 clause 4(2)(b)	Ex officio
Mr Martin CLIFFORD	Nominee of the Regional Minister Schedule 2 clause 4(3)	14/10/2015
Ms Elizabeth TAYLOR	Community representative Schedule 2 clause 4(2)(d)	05/11/2014
Mr Ian HOLLOWAY	Professions representative Schedule 2 clause 4(2)(e)	05/11/2014
Vacant	Local government representative Schedule 2 clause 4(2)(f)	
Ms Megan BARTLE	WAPC appointee Schedule 2 clause 4(2)(g)	05/11/2014
Ms Sue BURROWS	WAPC appointee Schedule 2 clause 4(2)(g)	05/11/2014

Quorum: 4

In accordance with the Western Australian Planning Commission (WAPC) Standing Orders 2009, 3.7 - Quorum for meetings:

(2) A quorum for a meeting of a committee is at least 50% of the number of offices (whether vacant or not) of members of the committee.

Role:

The Statutory Planning Committee is one of four committees set up by the WAPC on 1 March 1995 upon proclamation of the *Planning Legislation Amendment Act (No. 2) 1994*.

Schedule 2(4)(4) of the *Planning and Development Act 2005*

The Statutory Planning Committee is the WAPC's regulatory decision-making body and performs such of the functions of the WAPC under the *Planning and Development Act 2005* and Part II of the *Strata Titles Act 1985* as are delegated to the Statutory Planning Committee under section 16 and such other functions as are delegated to it under that section. These functions include approval of the subdivision of land, approval of leases and licenses, approval of strata schemes, advice to the Minister for Planning on local planning schemes and scheme amendments, and the determination of certain development applications under the Metropolitan Region Scheme.

Delegated Authority (Del 2009/05)

Planning and Development Act 2005 Section 16(1)

- 2.1 Power to determine applications for approval to commence and carry out development lodged with or referred to the WAPC pursuant to the provisions of a region scheme.
- 2.2 Power to approve detailed plans requiring the subsequent approval of the WAPC as a condition of development approval pursuant to the provisions of a region scheme and power to confirm that conditions imposed by the WAPC on a development approval pursuant to the provisions of a region scheme have been complied with.
- 2.3 Power to determine whether or not proposals and the ongoing implementation of a region scheme comply with conditions (if any) applied pursuant to sections 48F and 48J of the *Environmental Protection Act 1986*.
- 2.4 Power to determine whether or not applications to commence and carry out development are of State or regional importance, or in the public interest, pursuant to any resolution of the WAPC made under a region scheme requiring such determination.
- 2.5 Power to request the Minister for Planning to approve the WAPC disregarding the advice of the Swan River Trust in whole or in part in relation to the approval of development of land within the Riverbank or Development Control Area as defined under the *Swan and Canning Rivers Management Act 2006* where the determining authority is the WAPC.
- 2.6 All functions of the WAPC as set out in -
 - (i) Sections 14(a), 14(c), 34, 97, 98, 100, 104, 105, 106, 107, 109, 110, 111, 134, 135, 136, 138, 139, 140, 142, 143, 144, 145, 147, 151, 153, 154, 157, 169, 185, 214, 215, 216 of the Act;
 - (ii) Town Planning Regulations 1967;
 - (iii) Regulations 21, 22, 24 and 27 of the Planning and Development Regulations 2009;
 - (iv) *Strata Titles Act 1985* or the provisions of a strata or survey-strata scheme;
 - (v) Strata Titles General Regulations 1996;
 - (vi) Section 52 and section 85 of the *Land Administration Act 1997*;
 - (vii) Section 40 of the *Liquor Control Act 1988*;
 - (viii) *Perry Lakes Redevelopment Act 2005*.
- 2.7 Power to determine requests for variations to plans of subdivision where WAPC approval is required pursuant to the provisions of an approved local planning scheme.
- 2.8 Power to provide comment on and grant approval to plans known generally as outline development plans, structure plans and similar plans, and to planning policies and similar documents or amendments thereto, requiring the approval or endorsement of the WAPC pursuant to the provisions of a local planning scheme.
- 2.9 Power to provide comments or advice on behalf of the WAPC to a local government or a redevelopment authority where a provision of a local planning scheme or a redevelopment scheme requires comments from the WAPC.
- 2.10 Power to execute and accept the benefit of easements in gross, covenants in gross, records on title and other instruments for dealings in land for subdivisions, strata subdivisions and developments in accordance with any applicable policy and legislation.

- 2.11 Power to make recommendations to the Minister for Planning in relation to requests from local governments to expend monies paid by subdividing land owners in lieu of setting aside free of cost to the Crown, areas of land for public open space, where such recommendations are in accordance with WAPC policy.
- 2.12 Power to determine whether or not a proposal is likely to have a significant effect on the environment pursuant to section 38(1) of the *Environmental Protection Act 1986* and to refer such proposal to the Environmental Protection Authority.
- 2.13 Power to waive or clear conditions affixed as conditions of approval.
- 2.14 Power to endorse diagrams and plans of survey and deposited plans involving the acquisition and resumption of land created pursuant to Part 11 of the Act and the *Land Administration Act 1997*.
- 2.15 Power to advise the Minister for Planning on any appeal or matter arising therefrom pursuant to Part 14 of the Act.
- 2.16 Power to defend and otherwise deal with applications for review lodged with the Administrative Tribunal and to appeal, defend, respond and otherwise deal with any matter that may be appealed to the Supreme Court on a question of law.
- 2.17 Power to defend, respond, appeal and otherwise deal with legal proceedings.
- 2.18 Power to prepare and approve, subject to the prior approval of the Minister for Planning, policies relating to planning matters and/or the functions of the WAPC, save and except for State Planning Policies under Part 3 of the Act.
- 2.19 Power to determine matters under Regional Interim Development Orders.
- 2.20 Such powers and functions of the WAPC as set out in-
- (i) Part 5 of the Act;
 - (ii) *Town Planning Regulations 1967*
- as are necessary for the preparation, promulgation and the making of recommendations in relation to the Improvement Scheme authorised by Improvement Plan No. 37 for the Browse Liquefied Natural Gas Precinct.

This meeting is not open to members of the public.

RELEVANT INFORMATION FOR MEMBERS

Disclosure of interests

In accordance with the *Planning and Development Act 2005* and Part 6 of the Standing Orders 2009, members of Committees (and certain employees) are required to disclose the following types of interests that they have or persons closely associated to them, have:

- direct and indirect pecuniary interests (financial);
- proximity interests (location); and
- impartiality interests (relationship).

A “**direct pecuniary interest**” means a relevant person’s interest in a matter where it is reasonable to expect that the matter if dealt with by the board or a Committee, or an employee in a particular way, will result in a financial gain, loss, benefit or detriment for the person.

An “**indirect pecuniary interest**” means a relevant person’s interest in a matter where a financial relationship exists between that person and another person who requires a board or Committee decision in relation to the matter.

A “**proximity interest**” means a relevant person’s interest in a matter if the matter concerns -

- (i) a proposed change to a planning scheme affecting land that adjoins the person’s land;
- (ii) a proposed change to the zoning or use of land that adjoins the person’s land; or
- (iii) a proposed development, maintenance or management of the land or of services or facilities on the land that adjoins the person’s land.

An “**Impartiality interest**” means an interest that could, or could reasonably be perceived to, adversely affect the impartiality of the member having the interest and includes an interest arising from kinship, friendship, partnership or membership of an association or an association with any decision making process relating to a matter for discussion before the board or a Committee.

Members disclosing any pecuniary or proximity interests for an item can not participate in discussion or the decision making procedure relating to the item and must leave the meeting room during the discussion of the item. Members disclosing an impartiality interest in an item must also leave the room during the discussion or the decision making procedure relating to the item unless the Committee, by formal resolution, allows the member to remain. The reason to allow a member to remain must be stated in the formal resolution and will be minuted.

Disclosure of representations

Where a member has had verbal communication with or on behalf of a person with an interest in a matter which is before a meeting, the member is to disclose the interest.

Where a member is in receipt of relevant written material (including email) from or on behalf of a person with an interest in a matter which is before a meeting, the member is to table the material at the meeting for the information of members and relevant employees.

ORDER OF BUSINESS

- 1. Declaration of opening**
- 2. Apologies:**
- 3. Members on leave of absence and applications for leave of absence**
- 4. Disclosure of interests**
- 5. Declaration of due consideration**
- 6. Deputations and presentations**
- 7. Announcements by the Chairperson of the board and communication from the WAPC**
- 8. Confirmation of minutes of the meeting of 11 March 2014**
- 9. Reports (see attached index of reports)**
- 10. Confidential items (see attached index of reports)**
- 11. General business**
- 12. Items for consideration at a future meeting**
- 13. Closure - next Ordinary meeting to be held on 8 April 2014 and next Policy meeting will be held on 22 April 2014**

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of ordinary meeting 7497
held on Tuesday 11 March 2014

Attendance

Members

Mr Eric Lumsden	WAPC Chairman (Presiding Member) (arrived 9.20 am)
Ms Sue Burrows	WAPC appointee
Mr Martin Clifford	Regional Minister's nominee (Deputy)
Mr Ian Holloway	Professions representative (arrived 9.04 am)
Mr Duncan Ord	Acting Director General, Department of Planning
Ms Elizabeth Taylor	Community representative (Presiding Member) (to 9.20 am)

Officers

Ms Pam Baskind	Department of Planning Planning Manager, Regional Planning and Strategy (Item 9.3)
Ms Stephanie Clegg	Senior Planning Officer, Regional Planning and Strategy (Item 9.3)
Mr Andrew Cook	Senior Planning Officer, Perth and Peel Planning (Item 9.4)
Mr Robert Cull	Senior Planning Officer, Perth and Peel Planning (Item 9.4)
Mr Ken Dawson	Planning Manager, Regional Planning and Strategy (Item 10.5)
Ms Rebecca Fuller	Senior Planning Officer, Schemes and Amendments (Item 9.4)
Ms Kym Petani	Director Metro North West / Acting Director Metro North East (Item 9.4)
Ms Elisabeth Tamouridou	Planning Officer, Metro Planning (Item 9.4)
Mr Emille van Heyningen	Planning Manager, Perth and Peel Planning (Item 9.4)
Mr Philip Woodward	Planning Director, Regional Planning and Strategy (Item 10.5)

Committee Support

Ms Christina Sanders	Committee Support Officer - Department of Planning
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7497.1 Declaration of Opening

Due to the absence of the Presiding Member and in accordance with clause 3.6 of the Standing Orders 2009, a member was chosen to preside over the meeting as the Chairman would be unavoidably late to the meeting.

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**Mr Ord nominated Ms Taylor
Ms Taylor accepted the nomination.**

Resolved

Moved by Mr Ord, seconded by Ms Burrows

That Ms Taylor preside over the meeting of the Statutory Planning Committee in the absence of the Presiding Member.

The motion was put and carried.

The Presiding Member declared the meeting open at 9.00 am, acknowledged the traditional owners and custodians of the land on which the meeting is taking place and welcomed Members.

7497.2 Apologies

Ms Megan Bartle WAPC appointee

7497.3 Members on Leave of Absence and Applications for Leave of Absence

Nil.

7497.4 Disclosure of Interests

Nil.

7497.5 Declaration of Due Consideration

No declarations were made.

7497.6 Deputations and Presentations

Nil.

Moved to item 8.

7497.7 Announcements by the Chairperson of the Board and communication from the WAPC

Held over pending arrival of the WAPC Chairman. (There were subsequently no announcements.)

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7497.8 Confirmation of Minutes

7497.8.1 Minutes of the Statutory Planning Committee meeting held on Tuesday 25 February 2014

Resolved

Moved by Mr Clifford, seconded by Ms Burrows

That the minutes of the Statutory Planning Committee meeting held on Tuesday 25 February 2014, be confirmed as a true and correct record of the proceedings.

The motion was put and carried.

7497.9 Reports

Members agreed to endorse en bloc, the recommendations associated with Items 7497.9.1 and 9.2.

Resolved

Moved by Mr Ord, seconded by Ms Burrows

That the Western Australian Planning Commission resolves to endorse, en bloc, recommendations 9.1 and 9.2.

The motion was put and carried.

Members agreed to endorse en bloc, the recommendations associated with Items 7497.9.5 to 9.8, 10.4 and 10.6.

Resolved

Moved by Mr Clifford, seconded by Mr Ord

That the Western Australian Planning Commission resolves to endorse, en bloc, recommendations 9.5 to 9.8, 10.4 and 10.6.

Moved to Item 9.3.

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7497.9.1 City of Greater Geraldton R-Codes Policy to Vary State Planning Policy 3.1 Residential Design Codes

File 857/3/21/1PV
Report Number SPC/608
Agenda Part A
Reporting Officer Manager - Statutory Planning, Central Regions

Resolved

Moved by Mr Ord, seconded by Ms Burrows

That the Western Australian Planning Commission resolves to:

- 1. approve the amendment of deemed-to-comply provisions of State Planning Policy 3.1 Residential Design Codes as contained in the City of Greater Geraldton Outbuildings Local Planning Policy, adopted by the City of Greater Geraldton on 26 November 2013;*
- 2. notify the local government accordingly.*

The motion was put and carried.

7497.9.2 City of Kalgoorlie-Boulder R-Codes Policy (Rcodes01) to Vary State Planning Policy 3.1 Residential Design Codes

File TPS/0458
Report Number SPC/609
Agenda Part A
Reporting Officer Manager - Statutory Planning, Central Regions

Resolved

Moved by Mr Ord, seconded by Ms Burrows

That the Western Australian Planning Commission resolves to:

- 1. approve the amendment of deemed-to-comply provisions of State Planning Policy 3.1 Residential Design Codes as contained in the City of Kalgoorlie-Boulder Residential Design Codes Policy RCODES01*

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*(Attachment 1), adopted by the City of
Kalgoorlie-Boulder on 16 December 2013;*

2. *notify the local government accordingly.*

The motion was put and carried.

7497.9.3

Indian Ocean Drive Planning Guideline – Final Approval

File DP/12/00980
Report Number SPC/
Agenda Part A
Reporting Officer Strategic Planning Manager, Wheatbelt
Region

Ms Clegg and Ms Baskind provided further information in answer to members' queries.

Resolved

Moved by Mr Holloway, seconded by Ms Burrows

*That the Western Australian Planning
Commission resolves to:*

1. *determine the submissions in accordance with the attached schedule of submissions;*
2. *endorse the Indian Ocean Drive Planning Guideline subject to minor editing and publish the document on the Western Australian Planning Commission website;*
3. *prior to publication, forward the Guideline to the Minister for Planning for noting.*

The motion was put and carried.

7497.9.4

Consideration of WAPC Modification 38(I) to the North Alkimos Local Structure Plan No. 73

File SPN/0012
Report Number SPC/611
Agenda Part C
Reporting Officer Senior Planning Officer, Metro Planning
North West

Resolved

Moved by Mr Ord, seconded by Mr Holloway

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That the Western Australian Planning Commission resolves to:

- 1. re-affirm, in part, the inclusion of Modification 38(i) in its 10 November 2009 Schedule of Modifications, requiring the transit precinct to be included in the Centre Zone;*
- 2. advise the City of Wanneroo accordingly.*

The motion was put and carried.

Moved to Item 10.1.

7497.9.5 Proposed Subdivision – Lots 323 And 324 (79) Rupert Street, Subiaco

File 148979
Report Number SPC/612
Agenda Part G
Reporting Officer Planning Officer, Metropolitan Central, Perth and Peel Planning

Resolved

Moved by Mr Clifford, seconded by Mr Ord

That the Western Australian Planning Commission resolves to approve the application for amalgamation and resubdivision of Lots 323 and 324 Rupert Street, Subiaco as shown on the plan date stamped 30 October 2013 subject to the following conditions and advice:

CONDITIONS:

- 1. Other than buildings, outbuildings and/or structures shown on the approved plan for retention, all buildings, outbuildings and/or structures present on lots 323 and 324 at the time of subdivision approval being demolished and materials removed from the lots. (Local Government).*
- 2. Arrangements being made to the satisfaction of the Western Australian Planning Commission and to the specification of Western Power for the provision of an underground electricity*

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supply to the lots shown on the approved plan of subdivision. (Western Power).

3. *Arrangements being made with the Water Corporation so that provision of a suitable water supply service will be available to the lots shown on the approved plan of subdivision. (Water Corporation).*
4. *Arrangements being made with the Water Corporation so that provision of a sewerage service will be available to the lots shown on the approved plan of subdivision. (Water Corporation).*
5. *Suitable arrangements being made with the Water Corporation for the drainage of the land either directly or indirectly into a drain under the control of that body. (Water Corporation).*
6. *All septic sewer systems including all tanks and pipes and associated drainage systems (soak wells or leach drains) and any stormwater disposal systems are to be decommissioned, in accordance with the Health (Treatment of Sewerage and Disposal of Effluent and Liquid Waste) Regulations 1974, removed, filled with clean sand and compacted. Proof of decommissioning is to be provided in the form of either certification from a licensed plumber or a statutory declaration from the landowner/applicant, confirming that the site has been inspected and all septic tanks, soak wells, leach drains and any associated pipework have been removed. (Local Government).*

ADVICE:

1. *In regard to Condition 1, planning approval and/or a demolition licence may be required to be obtained from the local government prior to the commencement of demolition works.*

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2. *In regard to Condition 2, Western Power provides only one underground point of electricity supply per freehold lot.*
3. *In regard to Conditions 3, 4 and 5, the landowner/applicant shall make arrangements with the Water Corporation for the provision of the necessary services. On receipt of a request from the landowner/applicant, a Land Development Agreement under Section 67 of the Water Agencies (Powers) Act 1984 will be prepared by the Water Corporation to document the specific requirements of the proposed subdivision.*

The motion was put and carried.

7497.9.6

Subdivision to Create Three Survey Strata Lots for Residential Purpose at Lot 355 Solas Road, Morley

File 1324-13
Report Number SPC/613
Agenda Part G
Reporting Officer Planning Officer - Metropolitan Planning Central

Resolved

Moved by Mr Clifford, seconded by Mr Ord

That the Western Australian Planning Commission resolves to approve the application for survey-strata of Lot 355 Solas Road, Morley as shown on the plan date stamped 11 October 2013 subject to the following conditions and advice:

CONDITION(S):

1. Other than buildings, outbuildings and/or structures shown on the approved plan for retention, all buildings, outbuildings and/or structures present on lot(s) at the time of subdivision approval being demolished and materials removed from the lot(s). (Local Government)
2. All septic sewer systems including all tanks and pipes and associated drainage

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systems (soak wells or leach drains) and any stormwater disposal systems are to be decommissioned, in accordance with the Health (Treatment of Sewerage and Disposal of Effluent and Liquid Waste) Regulations 1974, removed, filled with clean sand and compacted. Proof of decommissioning is to be provided in the form of either certification from a licensed plumber or a statutory declaration from the landowner/applicant, confirming that the site has been inspected and all septic tanks, soak wells, leach drains and any associated pipework have been removed. (Local Government)

3. Pursuant to Section 150 of the *Planning and Development Act 2005* and Division 3 of the *Planning and Development Regulations 2009* a covenant preventing vehicular access onto Morley Drive East being lodged on the certificate(s) of title of the proposed lot(s) at the full expense of the landowner/applicant. The covenant is to prevent access, to the benefit of the Western Australian Planning Commission, in accordance with the plan dated 11 October 2013 (attached) and the covenant is to specify:

"No vehicular access is permitted from Morley Drive East." (Western Australian Planning Commission)

4. Redundant vehicle crossover(s) to be removed and the kerbing, verge, and footpath (where relevant) reinstated with grass or landscaping to the satisfaction of the Western Australian Planning Commission and to the specifications of the local government. (Local Government)
5. Suitable arrangements being made with the local government for the provision of vehicular crossover(s) to service the lot(s) shown on the approved plan of subdivision. (Local Government)

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6. Arrangements being made with the Water Corporation so that provision of a suitable water supply service will be available to the lots shown on the approved plan of subdivision. (Water Corporation)
7. Arrangements being made with the Water Corporation so that provision of a sewerage service will be available to the lots shown on the approved plan of subdivision. (Water Corporation)
8. Suitable arrangements being made with the Water Corporation for the drainage of the land either directly or indirectly into a drain under the control of that body. (Water Corporation)
9. Arrangements being made to the satisfaction of the Western Australian Planning Commission and to the specification of Western Power, for the provision of an electricity supply to the survey strata lots shown on the approved plan of subdivision, which may include the provision of necessary service access rights either as an easement under Section 136C and Schedule 9A of the *Transfer of Land Act 1893* for the transmission of electricity by underground cable, or (in the case of approvals containing common property) via a portion of the common property suitable for consumer mains. (Western Power)

ADVICE:

1. In regard to Condition 1, planning approval and/or a demolition licence may be required to be obtained from the local government prior to the commencement of demolition works.
2. In regard to Conditions 6, 7 and 8, the landowner/applicant shall make arrangements with the Water Corporation for the provision of the necessary services. On receipt of a request from the landowner/applicant, a Land

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Development Agreement under Section 67 of the *Water Agencies (Powers) Act 1984* will be prepared by the Water Corporation to document the specific requirements for the proposed subdivision.

3. In regard to Condition 9, Western Power provides only one underground point of electricity supply per freehold lot.

The motion was put and carried.

7497.9.7

Development to Create One Grouped Dwelling and Four Multiple Dwellings - Lot 174 (No. 42) Winchelsea Road, Nollamara

File 20-50468-1
Report Number SPC/614
Agenda Part G
Reporting Officer Planning Officer - Metropolitan Planning Central

Resolved

Moved by Mr Clifford, seconded by Mr Ord

That the Western Australian Planning Commission resolves to refuse the proposed development of one grouped dwelling and four multiple dwellings at Lot 174 (No.42) Winchelsea Road, Nollamara as the application does not comply with section 6.3.3 of State Planning Policy 3.1 Residential Design Codes in respect to the provision of on-site car parking.

The motion was put and carried.

7497.9.8

Approval to Commence Development for a Public Transport Authority Bus Depot - Lot 8278 Ocean Reef Road, Craigie

File 34-50105-5
Report Number SPC/615
Agenda Part G
Reporting Officer Planning Officer, Metro Planning North West

Resolved

Moved by Mr Clifford, seconded by Mr Ord

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That the Western Australian Planning Commission resolves to approve the application to commence development for a Public Transport Authority bus depot at Lot 8278, Ocean Reef Road, Craigie, subject to the following conditions and advice:

CONDITIONS

1. *This approval relates specifically to the attached plans date-stamped 15 November 2013 by the Department of Planning, on behalf of the Western Australian Planning Commission.*
2. *A noise barrier with a minimum height of 3.0 metres being provided along the western and northern boundaries of the application site, in the location shown on the attached plan, to the specifications of the Department of Environment Regulation, and to the satisfaction of the Western Australian Planning Commission.*
3. *A Construction Management Plan being prepared and implemented to the specification of the City of Joondalup and to the satisfaction of the Western Australian Planning Commission.*
4. *A Site Management Plan being prepared and implemented to address the issues of noise, lighting, dust/odour, and any other matters arising from the depot operation likely to impact on the surrounding residences, to the specifications of the City of Joondalup and to the satisfaction of the Western Australian Planning Commission.*
5. *Detailed landscaping plans for the site being prepared and approved by the City of Joondalup prior to the commencement of development. Landscaping and reticulation shall be established in accordance with the approved plans prior to the occupancy of the development and shall thereafter be maintained to the specification of the City*

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of Joondalup and to the satisfaction of the Western Australian Planning Commission.

6. *An on-site stormwater drainage system with the capacity to contain a 1:100 year storm of 24-hour duration, is to be provided prior to the development first being occupied and thereafter maintained to the specification of the City of Joondalup and to the satisfaction of the Western Australian Planning Commission.*
7. *The car parking areas, driveways and points of ingress and egress shall be designed in accordance with the requirements of the Australian Standard for Off-street Car Parking (AS 2890). Such areas to be constructed, drained and marked prior to the practical completion of the development and thereafter maintained to the specification of the City of Joondalup and to the satisfaction of the Western Australian Planning Commission.*
8. *The car parking area is to be provided with one shade tree for every four (4) bays prior to the development first being occupied and thereafter maintained to the specification of the City of Joondalup and to the satisfaction of the Western Australian Planning Commission.*
9. *The access street/Ocean Reef Road intersection being modified to accommodate right and left turn vehicle movements onto Ocean Reef Road, to the specifications of Main Roads WA and the City of Joondalup, and to the satisfaction of the Western Australian Planning Commission.*

ADVICE TO APPLICANT

1. *All development must comply with the provisions of the Health Regulations, Building Code of Australia, Public Building Regulations and all other relevant Acts, Regulations and Local Laws. This includes*

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the provisions of access and facilities for people with disabilities in accordance with the Building Codes of Australia.

2. *With regard to Condition 3, the City of Joondalup advises that the Construction Management Plan shall address the following:*
 - (a) all forward works for the site;*
 - (b) the delivery of materials and equipment to the site;*
 - (c) the storage of materials and equipment on the site;*
 - (d) the parking arrangements for the contractors and subcontractors;*
 - (e) the management of dust during the construction process;*
 - (f) any other matters likely to impact on the surrounding properties.*

3. *With regard to Condition 9, Main Roads WA advises as follows:*
 - (a) The developer shall be responsible for all costs involved in the design and construction of the intersection and access. This includes signing, road markings, relocation of services, street lighting and Main Roads costs involved in the checking of the design and constructions drawings and any site inspections.*
 - (b) Main Roads approval for the construction drawings is required before any work is undertaken within the Mitchell Freeway reservation. A detailed traffic management safety plan while working within the road reservation is to be submitted as part of this approval.*
 - (c) All enquiries shall be directed to the Metropolitan Region Asset Manager (North).*

4. *With regard to the Mitchell Freeway road reserve:*

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- (a) *no earthworks shall encroach onto the reserve;*
- (b) *no stormwater drainage shall be discharged into the reserve;*
- (c) *the applicant shall make good any damage to the existing verge vegetation within the reserve; and*
- (d) *the ground levels of the freeway boundary are to be maintained as existing.*

5. *ATCO Gas Australia has high pressure gas mains in the vicinity of the property, and a high pressure gas service on the property. ATCO Gas must be notified of any works within 15 metres of high pressure gas infrastructure before those works begin. Construction, excavation and other activities may be restricted in this zone. No pavements (including crossovers) are to be constructed over any pipeline without consent from ATCO Gas Australia. Various pipeline safety tests may apply. The applicant is advised to contact ATCO in this regard.*

The motion was put and carried.

The Chairman arrived at 9.20 am and resumed the Chair.

7497.10 Confidential Items

7497.10.1 **City of Swan - Local Planning Scheme Amendment No. 90 - For Final Approval**

File TPS/1089/1
Report Number SPC/616
Agenda Part B
Reporting Officer A/Director - Schemes, Strategies and Amendments

THIS ITEM IS CONFIDENTIAL

7497.10.2 **City of Stirling Local Planning Scheme No.3 Amendment No.32 - Consent to Advertise**

File TPS/0973
Report Number SPC/617
Agenda Part B
Reporting Officer A/Director - Schemes and Amendments

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7497.10.3 Shire of Ashburton Local Planning Scheme Amendment No.24 - For Final Approval

File TPS/1022/1
Report Number SPC/618
Agenda Part B
Reporting Officer Director Planning, Northern Regions
Principal Planning Officer, Pilbara

THIS ITEM IS CONFIDENTIAL

7497.10.4 Shire of Broomehill Tambellup Local Planning Strategy - Request for Consent to Advertise

File TPS/0465/1
Report Number SPC/619
Agenda Part E
Reporting Officer Planning Officer

THIS ITEM IS CONFIDENTIAL

7497.10.5 Shire of Esperance - Local Planning Scheme No.23 Amendment 14 - For Final Approval

File TPS/1174/1
Report Number SPC/620
Agenda Part E
Reporting Officer Planning Manager - Central Regions

THIS ITEM IS CONFIDENTIAL

7497.10.6 Shire of Shark Bay Local Planning Scheme No. 4 - Resolution to Prepare a Scheme

File TPS/1301/1
Report Number SPC/621
Agenda Part E
Reporting Officer Planning Manager - Central Regions

THIS ITEM IS CONFIDENTIAL

7497.11 General Business

Nil.

Statutory Planning Committee

Minutes
of ordinary meeting 7497
held on Tuesday 11 March 2014

7497.12 Items for Consideration at a Future Meeting

Nil.

7497.13 Closure

The next ordinary meeting is scheduled for 10.00 am on Tuesday 25 March 2014.

There being no further business before the Committee, the Presiding Member thanked members for their attendance and declared the meeting closed at 10.00 am.

PRESIDING MEMBER _____

DATE _____

Unconfirmed

INDEX OF REPORTS

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9	REPORTS
C	SUBDIVISIONAL / AMALGAMATIONS
9.1	Subdivision of Lot 241 Phyllis Street, North Fremantle
D	GENERAL ITEMS / OTHER MATTERS
9.2	Development Application - Hard and Soft Landscaping Treatments for Perth Waterfront Project
G	DEVELOPMENTS / SUBDIVISIONAL / SURVEY STRATA
9.3	Four Multiple Dwellings - Lot 1 (No. 12) Cobham Avenue, Nollamara
9.4	One Grouped Dwelling and Four Multiple Dwellings - Lot 4 (No. 10) Vermont Street, Nollamara
9.5	Proposed Mobile Asphalt Processing Plant
10	CONFIDENTIAL REPORTS
A	POLICY
10.1	Rural Planning Guidelines Update
10.2	Liveable Neighbourhoods (LN) Review – Project Status Update
C	SUBDIVISIONS / AMALGAMATIONS
10.3	Reconsideration of Modifications Required to the City of Swan Local Structure Plan No. 11
D	GENERAL ITEMS / OTHER MATTERS
10.4	Shire of Carnarvon Coral Bay Settlement Structure Plan - Final Approval
E	MINOR LOCAL OR REGIONAL PLANNING SCHEMES / LOCAL PLANNING SCHEME AMENDMENTS
10.5	Shire of Toodyay Local Planning Scheme No. 4 Amendment No. 2 - For Final Consideration

ITEM NO: 9.1

Subdivision of Lot 241 Phyllis Street, North Fremantle

WAPC OR COMMITTEE:	Statutory Planning Committee
REPORTING AGENCY:	Department of Planning
REPORTING OFFICER:	Senior Planner, Metropolitan Planning Central
AUTHORISING OFFICER:	Director, Metropolitan Planning Central
AGENDA PART:	C
FILE NO:	148308
DATE:	12 March 2014
ATTACHMENT(S):	Attachment 1 Application Plan Attachment 2 Location Plan Attachment 3 Aerial View Attachment 4 Street View Photos Attachment 5 Expired Deposited Plan

RECOMMENDATION:

That the Western Australian Planning Commission resolves to approve the application for subdivision of Lot 241 (No. 6) Phyllis Street, North Fremantle as shown on the plan date-stamped 27 June 2013 subject to the following conditions:

- 1. The land being filled, stabilised, drained and/or graded as required to ensure that***
 - a) lots can accommodate their intended development; and***
 - b) finished ground levels at the boundaries of the lot(s) the subject of this approval match or otherwise coordinate with the existing and/or proposed finished ground levels of the land abutting; and***
 - c) stormwater is contained on-site, or appropriately treated and connected to the local drainage system (Local Government)***

SUMMARY:

The application is referred to the Committee for determination because of the specific circumstances that warrant consideration under section 138 (3) of the Planning and Development Act. The key points relating to this application are as follows:

- The application proposes to subdivide Lot 241 Phyllis Street, a R25 coded site, into 2 lots with areas of 253m²each, which are more consistent with the R40 coding;***

The street block comprises a mix of lot sizes and dwelling types including single houses and grouped dwellings and a variety of building styles (**Attachment 4 - Street View Photo**).

The land is currently zoned 'Residential R25'.

GOVERNMENT AND CORPORATE IMPLICATIONS:

Nil

CONSULTATION:

The application was referred by the WAPC to the Local Government (City of Fremantle) and servicing agencies (Water Corporation and Western Power). The City of Fremantle has recommended that the application be refused on the grounds that the proposal is inconsistent with the requirements of the Residential Design Codes in respect to the minimum and average site area requirements for properties with a density code of R25.

The Water Corporation and Western Power have recommended that the application be approved and do not require any servicing conditions because both of the proposed lots have already been serviced under the earlier WAPC approval.

The applicant prepared a written justification in support of the subdivision and this accompanied the application. The applicant submits that the application is the same as an earlier approval, which has since lapsed and puts forward the following points in support of the application:

1. The proposed blocks each have a road frontage (to Phyllis Street and to Alfred Road).
2. The majority of residential lot sizes in Phyllis Street are in the region of 250m²., apart from several large lots fronting the river at the eastern end of the Phyllis Street and Alfred Road as well as grouped dwellings owned by the Department of Housing (DoH).
3. The land the subject of this application was purchased in March 2008 for \$1,630,000, with an accompanying conditional subdivision approval (subdivision into two lots with areas of 253m² each.)
4. The subdivision conditions required removal of the existing residential house on the property, removal of sheds and outbuildings, creation of a sewage easement to serve the Alfred road block and under-grounding of power. All works associated with the conditions of approval were undertaken by the owners.
5. The total cost of complying with the subdivision conditions, and paying rates and taxes on the property has exceeded \$100,000 and all these costs will be wasted if the subdivision does not go ahead.
6. A Deposited Plan was endorsed by the WAPC and Landgate but an Application for the creation of new titles was not submitted and the Deposited Plan subsequently expired as did the WAPC subdivision approval.

-
7. Landgate failed to ensure that the owners were informed of the subdivision expiry.

OFFICER'S COMMENTS:

The application proposes lot sizes that do not accord with the minimum or average lot size requirements of the R25 code and proposes lot sizes more a kin with the R40 code, which is consistent with the earlier zoning of the land under the City of Fremantle LPS 3 (prior to the gazettal of the City of Fremantle LPS No. 4).

The WAPC has previously granted subdivision approval over the site when the land was coded R40. The landowner has demonstrated that all necessary steps were taken to implement the earlier subdivision approval and to obtain new titles but through a misunderstanding of process, the owner failed to submit an Application for New Titles before the Deposited Plan expired (**Attachment 5 Expired Deposited Plan**).

The applicant has gone to great lengths and expense to subdivide the lot including the demolition of a dwelling, site works and installation of services for two lots. If subdivision approval is not granted by the WAPC, the only option available to the applicant is to remove one set of services and to sell the land as a vacant site.

As discussed above, it is considered that the application can be assessed and recommended for approval under section 138 of the Planning and Development Act whereby the WAPC may give an approval that conflicts with the provisions of the local planning scheme if the approval is consistent with a region planning scheme that deals with substantially the same matter. To this end it is considered that there are sufficient grounds under section 138 of the *Planning and Development Act 2005* to support the application for the following reasons:

- The proposal accords with State level policy which supports the strategic increase in density and infill of existing urban land;
- The subject land is surrounded by development at a greater density than R25 and the majority of lots coded R25 have already been developed to a greater density than is permitted under that coding;
- The City of Fremantle Local Planning Scheme 4 is older than 5 years and is in need of review;
- The proposal would not be detrimental to the amenity of the locality nor contrary to orderly and proper planning; and
- The WAPC has approved two previous subdivision proposals for the subject land (WAPC Refs: 124042 and 133998).

CONCLUSION:

The question surrounding this application is whether it warrants consideration under section 138 of the Act in light of the previous approvals granted by the WAPC and the subdivisional works undertaken by the applicant, which has resulted in a vacant site that is serviced to accommodate two lots. Taking into account the down coding of the land from R40 to R25, the earlier subdivision approval issued by the WAPC and

the extensive subdivisional works undertaken by the applicant, it is recommended that the application be assessed against Section 138 of the Act.

It is considered that there are reasonable grounds to support this application and the creation of two lots that are smaller than permitted under the R25 zoning of the land and apply section 138 of the Act. Taking into account the special circumstances of this application, the history of approvals issued over the site and the suitability of the site for subdivision and development, the application if approved, would not establish a precedent for further subdivision of a similar nature.

Accordingly, on the basis of the discussion above, and the particular circumstances of the subject application, the proposal is recommended for conditional approval.

INTERESTS AND NOTIFICATIONS

SUBJECT	PURPOSE	STATUTORY REFERENCE	ORIGIN	LAND BURDENED	BENEFIT TO	COMMENTS
(33b)	EASEMENT (SEWERAGE)	SEC.167 OF THE P & D ACT REG. 33(b)	THIS PLAN	LOTS 800 & 801	WATER CORPORATION	

VER	AMENDMENT	AUTHORISED BY	DATE

TYPE FREEHOLD

PURPOSE SUBDIVISION

PLAN OF

**LOTS 800 & 801
& EASEMENT**

SSA YES/NO

DISTRICT NORTH FREMANTLE (SWAN) DPI FILE

TOWNSITE

LOCAL AUTHORITY CITY OF FREMANTLE

LOCALITY NORTH FREMANTLE

FORMER TENURE LOT 241 ON DP 139059 C/T 1110-813	ON INDEX BG34(2).07.16	FIELD BOOK 112545
--	---------------------------------------	----------------------

SCALE 1:250 @ A3

ALL DISTANCES ARE IN METRES

SURVEYOR'S CERTIFICATE - Reg 54

I, **D.K. McALIECE**, hereby certify that this plan is accurate and is a correct representation of the -
 (a) survey; and/or
 (b) calculations from measurements,
 undertaken for the purposes of this plan and that it complies with the relevant written law(s)
 in relation to which it is lodged.

LICENSED SURVEYOR		DATE
LOGGED	TYPE OF VALIDATION	SURVEY FIRM
DATE	FULL AUDIT	CARLTON SURVEYS
FEE PAID	LEGAL COMPONENT	LICENSED SURVEYORS
ASSESS No.	DOCKET PLAN / DIAGRAM	SUITE 4, 160 BURSWOOD ROAD
	CERTIFIED CORRECT	BURSWOOD 6100
	I.S.C.	TELEPHONE 9361 5358 FAX 9361 3457
	F.S.C.	APPROVED BY
		WESTERN AUSTRALIAN
		PLANNING COMMISSION
		FILE: 133998

IN ORDER FOR DEALINGS

SUBJECT TO

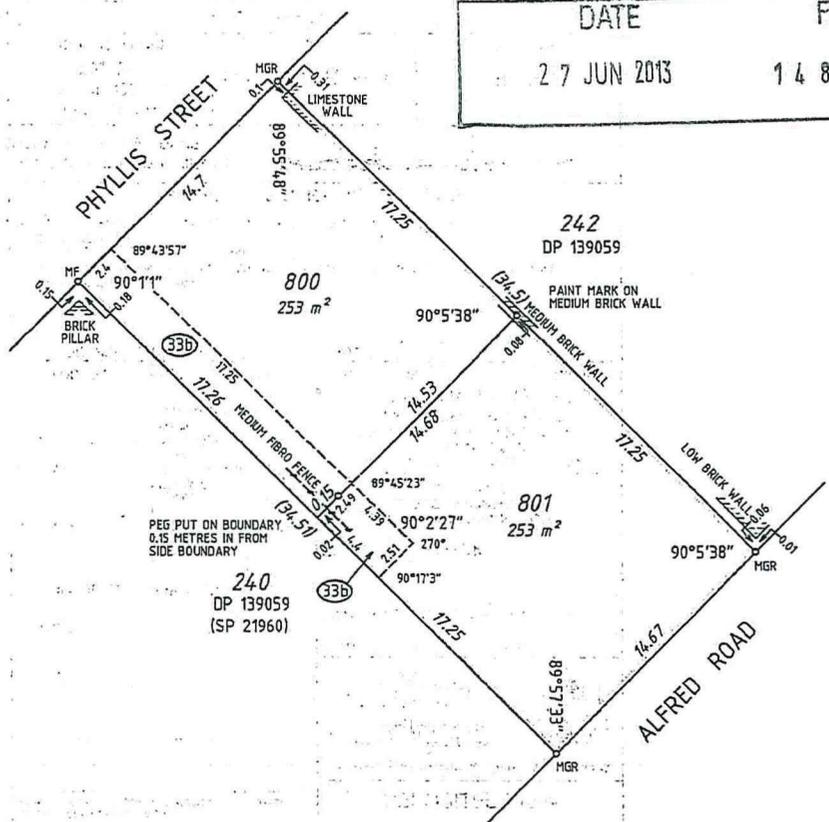
DELEGATED UNDER S.16 P & D ACT 2005

DATE

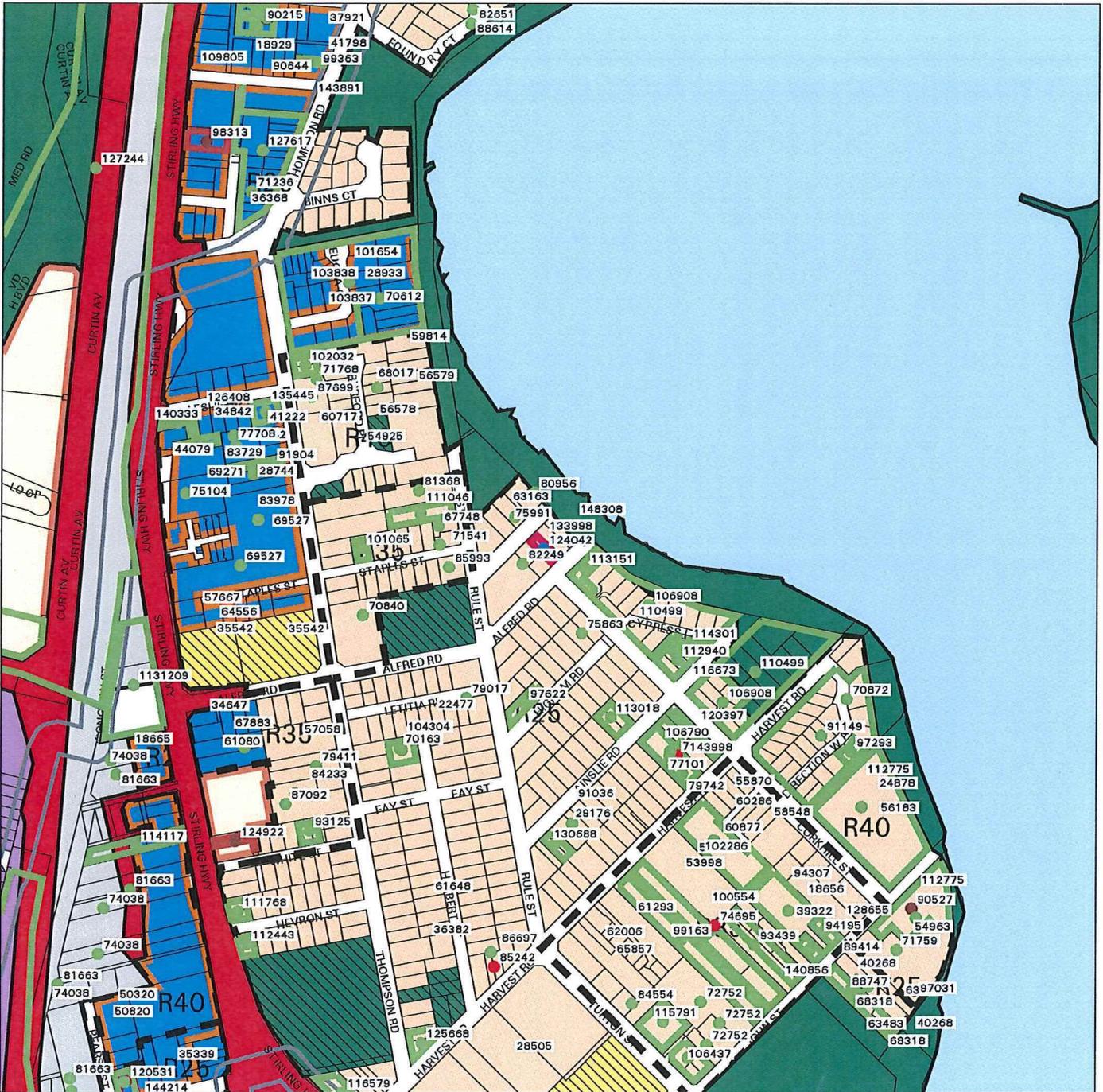
FOR INSPECTOR OF PLANS & SURVEYS AUTHORISED LAND OFFICER	DATE	 Landgate Western Australian Land Information Authority
APPROVED		
INSPECTOR OF PLANS & SURVEYS AUTHORISED LAND OFFICER	DATE	DEPOSITED PLAN <h1>65410</h1>
		SHEET 1 OF 1
		VERSION 1

DEPARTMENT OF PLANNING

DATE	FILE
27 JUN 2013	148308



ATTACHMENT 1



Subdivision Application 148308 (MGA ref 382579mE 6455586mN Zone 50)

This data is to be used for the processing of subdivision applications only.

- | | | | |
|--------------------------------|-----------------|--|------------|
| APPROX LOCATION OF APPLICATION | APPROVED | RESIDENTIAL | INDUSTRIAL |
| PRIMARY REGIONAL ROADS | CANCELLED | OPEN SPACE | |
| WATERWAYS | REFUSED | COMMUNITY FACILITIES
PRIMARY SCHOOL | |
| PARKS & RECREATION | OUTSTANDING | NO ZONE | |
| RAILWAYS | APPROVED | LOCAL CENTRE | |
| CADASTRAL BOUNDARY | CANCELLED | MIXED USE | |
| WESTNET ENERGY GAS PIPELINE | REFUSED | DEVELOPMENT | |
| OUTSTANDING | R CODE BOUNDARY | COMMUNITY FACILITIES
COMMUNITY FACILITIES | |

142500s.eps

- Legend**
- Cadastre (view 1)
 - Cadastre (view 2)
 - Cadastre (view 3)
 - Cadastre (view 4)

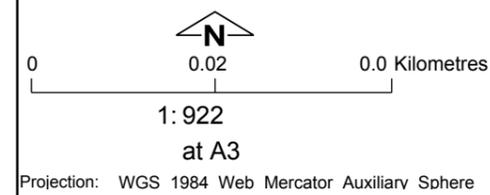


Application Area

PlanViewWA Map

INTERNAL USE ONLY

Internal Spatial Viewer
[Link to website](#)

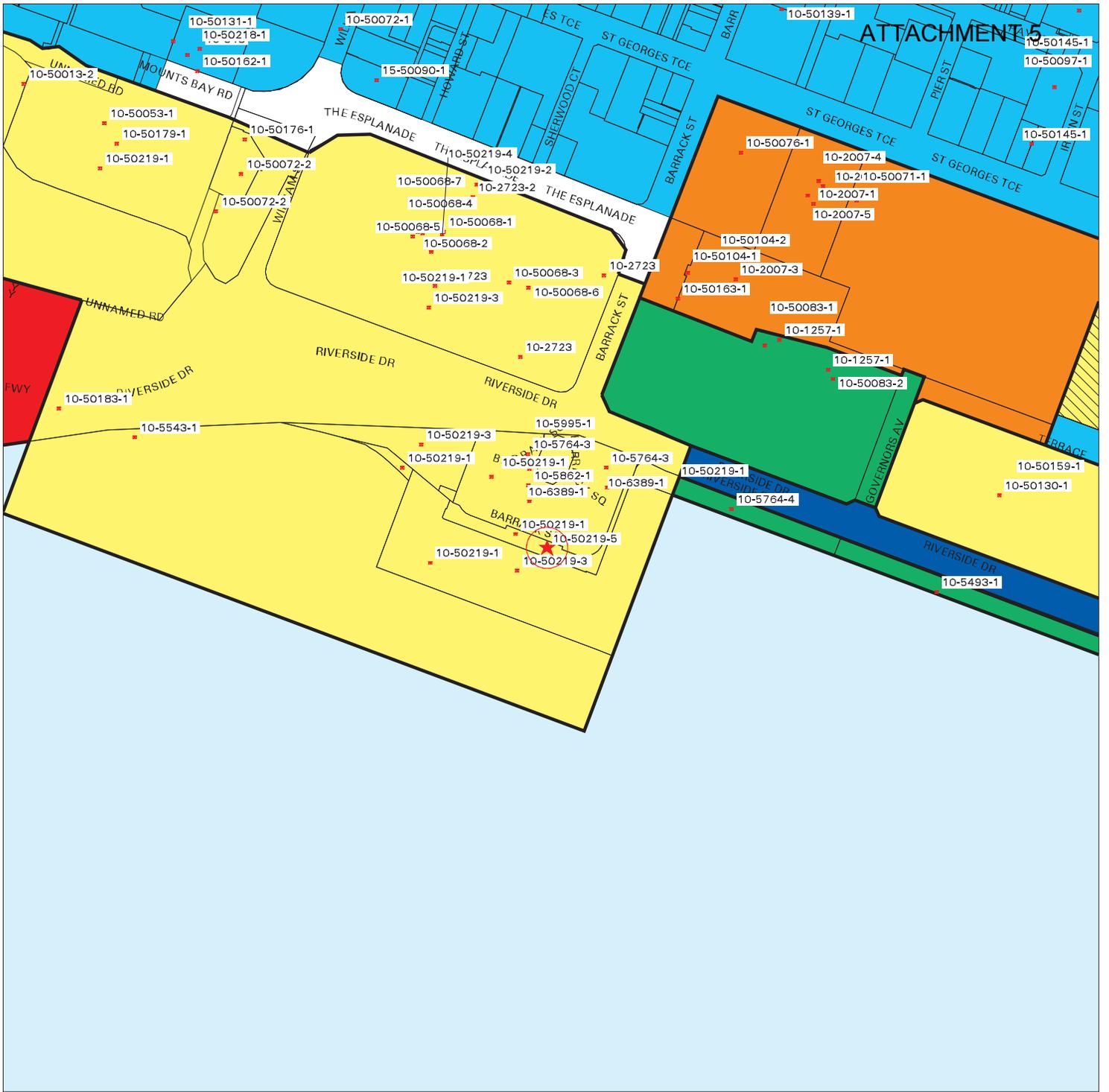


Notes:
 The data that appears on the map may be out of date, not intended to be used at the scale displayed, or subject to license agreements. This information is stored in the relevant layers metadata. For these reasons the map should not be distributed outside of the Department.
 Map was produced using DoP's PlanViewWA.

Produced by:
 Date produced: 13-Mar-2014



ATTACHMENT 4



Development Application 10-50219-5 (MGA ref 392092mE 6463460mN Zone 50)

This data is to be used for the processing of subdivision applications only.

-  APPROX LOCATION OF APPLICATION
-  PRIMARY REGIONAL ROADS
-  OTHER REGIONAL ROADS
-  WATERWAYS
-  PARKS & RECREATION
-  CIVIC & CULTURAL
-  PP - CAR PARK
-  CADASTRAL BOUNDARY
-  WESTNET ENERGY GAS PIPELINE
-  DEVELOPMENT APPLICATION
-  CITY CENTRE
-  PUBLIC PURPOSES CIVIC USE



INTERESTS AND NOTIFICATIONS						
SUBJECT	PURPOSE	STATUTORY REFERENCE	ORIGIN	LAND BURDENED	BENEFIT TO	COMMENTS
33b	EASEMENT (SEWERAGE)	SEC.167 OF THE P & D ACT REG. 33(b)	THIS PLAN	LOTS 800 & 801	WATER CORPORATION	

VER	AMENDMENT	AUTHORISED BY	DATE

TYPE FREEHOLD

PURPOSE SUBDIVISION

PLAN OF

**LOTS 800 & 801
& EASEMENT**

SSA YES/NO

DISTRICT NORTH FREMANTLE (SWAN) DPI FILE

TOWNSITE

LOCAL AUTHORITY CITY OF FREMANTLE

LOCALITY NORTH FREMANTLE

FORMER TENURE LOT 241 ON DP 139059 C/T 1110-813	ON INDEX .. BG34(2) 07.16	FIELD BOOK 112545
--	------------------------------------	----------------------

SCALE 1 : 250 @ A3
ALL DISTANCES ARE IN METRES

SURVEYOR'S CERTIFICATE - Reg 54

I, **D.K. McALIECE**, hereby certify that this plan is accurate and is a correct representation of the -
 (a) survey; and/or
 (b) calculations from measurements,
 undertaken for the purposes of this plan and that it complies with the relevant written law(s)
 in relation to which it is lodged.

Dion K McAlicee
Dion K McAlicee
2010.05.05 12:32:23 +08'00'

LOGGED	TYPE OF VALIDATION	SURVEY FIRM	JOB : 10516
DATE 6-May-10	FULL AUDIT	CARLTON SURVEYS	
FEE PAID \$316.00	LEGAL COMPONENT E.M.	LICENSED SURVEYORS	
ASSESS No. 6931503	DOCKET 448-2010/4	SUITE 4, 160 BURSWOOD ROAD	
	CERTIFIED CORRECT	BURSWOOD 6100	
	I.S.C. 20-5-10	TELEPHONE 9361 5358 FAX 9361 3457	
	F.S.C.	APPROVED BY	
		WESTERN AUSTRALIAN	
		PLANNING COMMISSION	
		FILE 133998	

IN ORDER FOR DEALINGS

SUBJECT TO

DELEGATED UNDER S.16 P & D ACT 2005

DATE

FOR INSPECTOR OF PLANS & SURVEYS DATE

AUTHORISED LAND OFFICER

APPROVED

INSPECTOR OF PLANS & SURVEYS DATE

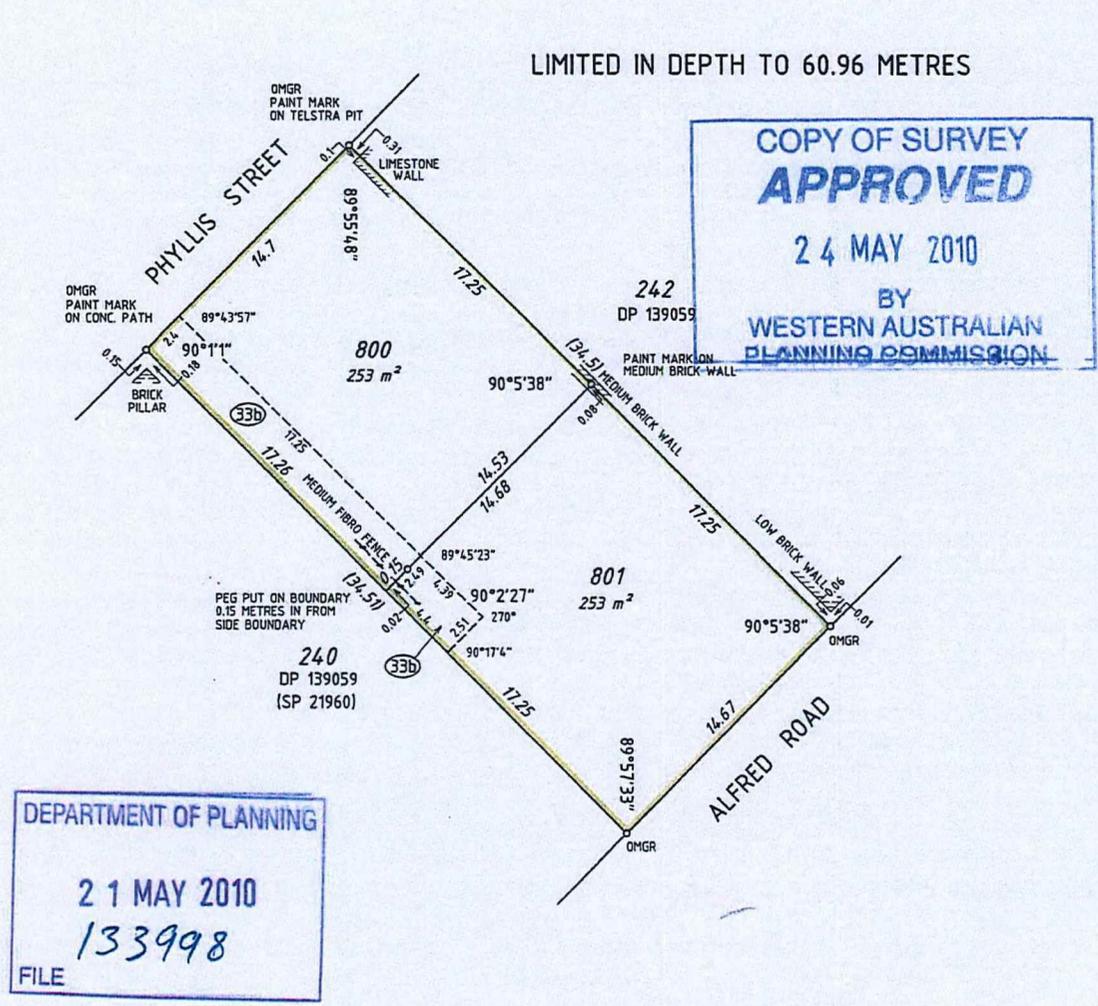
AUTHORISED LAND OFFICER

DEPOSITED PLAN

65410

SHEET 1 OF 1

VERSION 1



COPY OF SURVEY
APPROVED
 24 MAY 2010
 BY
WESTERN AUSTRALIAN
PLANNING COMMISSION

DEPARTMENT OF PLANNING
 21 MAY 2010
 133998
 FILE

ATTACHMENTS

ITEM NO: 9.2

Development Application - Hard and Soft Landscaping Treatments for Perth Waterfront Project

WAPC OR COMMITTEE: **Statutory Planning Committee**

REPORTING AGENCY: Department of Planning
REPORTING OFFICER: Senior Planner Metropolitan Planning Central
AUTHORISING OFFICER: Director Metropolitan Planning Central
AGENDA PART: D
FILE NO: 10-50219-5
DATE: 12 March 2014
ATTACHMENT(S): 1: Locality Plan
2: Elizabeth Quay Masterplan Area
3: Illustrative Ste Plan

RECOMMENDATION:

That the Western Australian Planning Commission resolves to approve this development application (DA3) for hard and soft landscaping treatments and associated works of the public realm areas for the Perth Waterfront project subject to the following condition(s) and advice:

- 1. This approval does not include the proposed installation of ten (10) courtesy moorings.***
- 2. The provision of easements pursuant to Section 167 of the Planning and Development Act 2005 for existing or planned power infrastructure being granted free of cost to Western Power.***
- 3. Arrangements being made to the satisfaction of the Western Australian Planning Commission and to the specification of Western Power for the removal, relocation and/or replacement of electricity supply infrastructure, including plant and/or equipment located on or near the development area.***
- 4. Any ground disturbing works conducted in the vicinity of identified contamination (including any contained contaminated fill material) being***

undertaken in accordance with the approved Site Management Plan for the Elizabeth Quay project.

5. *Any acid sulfate soils disturbing works being carried out in accordance with the Department of Environment Regulation approved Acid Sulfate Soil and Dewatering Management Plan for the Elizabeth Quay project.*
6. *Any changes to the approved Construction Management and Staging Plan for the Elizabeth Quay project shall be submitted in writing and approved by the Western Australian Planning Commission, on advice from relevant stakeholders, as appropriate, prior to the commencement or continuation of any alternative management method.*
7. *Prior to commencement of works relating to the island revetment, a Management Plan and Engineer's report for the rock revetment infrastructure being approved by the Western Australian Planning Commission upon advice of the City of Perth and the Metropolitan Redevelopment Authority.*
8. *Prior to commencement of works in relation to the relevant infrastructure and servicing for the public realm, details of infrastructure for a CCTV network for the Elizabeth Quay project that can be integrated into the City of Perth's existing CCTV network being approved by the Western Australian Planning Commission upon advice of the City of Perth and the Metropolitan Redevelopment Authority.*
9. *Prior to commencement of works in relation to the relevant infrastructure and servicing for the public realm, the design response to the accessibility report prepared by O'Brian Harris Access for Elizabeth Quay being approved by the Western Australian Planning Commission upon advice of the City of Perth.*
10. *Prior to commencement of works relating to the irrigation system for the public realm, details of the final irrigation plans and systems being approved by the Western Australian Planning Commission upon advice of the City of Perth and the Metropolitan Redevelopment Authority.*
11. *Prior to completion of the Elizabeth Quay works, Servicing and Loading Management Plans for events space, buildings, kiosks and the Florence Hummerston building being approved by the Western Australian Planning Commission upon advice of the City of Perth and the Metropolitan Redevelopment Authority.*
12. *Prior to completion of the Elizabeth Quay works, a Utilities Services Management Plan for the supply of services and connections to the island and for the infrastructure on the island being approved by the Western Australian Planning Commission upon advice of the City of Perth and the Metropolitan Redevelopment Authority.*

-
13. ***The applicant shall notify the Swan River Trust, in writing not less than seven (7) days prior to the commencement of works.***
 14. ***With the exception of the works proposed by this application, the applicant shall take all reasonable precautions to prevent inadvertent damage to the foreshore, riverbank, or waterway (including infrastructure, vegetation and sedimentation) as a result of the works. Should any damage occur, the applicant shall notify the Swan River Trust immediately.***
 15. ***Prior to the commencement of works relating to any urban water management activity, an Urban Water Management Plan shall be prepared by the applicant and approved by the Western Australian Planning Commission, on the advice of the Swan River Trust, City of Perth, Metropolitan Redevelopment Authority and Department of Water.***
 16. ***The approved Urban Water Management Plan referred to in Condition 15 above is to be implemented to the satisfaction of the Western Australian Planning Commission, on the advice of the Swan River Trust, City of Perth, Metropolitan Redevelopment Authority and Department of Water..***
 17. ***Prior to commencement of the pedestrian/cycle bridge structure works, the proponent shall prepare detailed design drawings for the bridge structure to the satisfaction of the Western Australian Planning Commission on advice of the Department of Transport.***
 18. ***Prior to commencement of the shared path and shared zone construction works, the proponent shall prepare detailed design drawings for shared zones and shared paths (in particular on the eastern side of William Street) to the satisfaction of the Western Australian Planning Commission on advice of the Department of Transport.***
 19. ***A detailed design audit for road works that involve vehicles, pedestrians and cyclists that are yet to be built and a post construction audit for work that involve vehicles, pedestrian and cyclists that have been completed or under construction the satisfaction of the Western Australian Planning Commission on the advice of the City of Perth, Department of Transport and Main Roads Western Australia.***
 20. ***Parking allocation for all sites is to be consistent with the City of Perth's Perth Parking Policy (2012).***
 21. ***Prior to the installation of the navigational aids for Elizabeth Quay, approval of the proposed navigation aids is to be obtained from the Maritime Safety Business Unit of Department of Transport.***
 22. ***Prior to construction of all proposed jetty structures, licences for the jetty structures are to be obtained from the Coastal Facilities section of the Department of Transport.***

-
23. ***This approval relates to the DA3 package submitted with the Western Australian Planning Commission dated 28 June 2012 and the 5th December 2013.***

Advice

1. ***In respect of Condition 1, the applicant is advised that the installation of ten additional swing moorings near the inlet will require further discussion with the Swan River Trust and separate consideration at a later date.***
2. ***In accordance with regulation 31(1)(c) of the Contaminated Sites Regulations 2006, a Mandatory Auditor's Report, prepared by an accredited contaminated sites auditor, will need to be submitted to the Department of Environment Regulation at the completion of works as evidence of compliance with Condition 4. A current list of accredited auditors is available from www.der.wa.gov.au/contaminatedsites/.***
3. ***In respect of Condition 4, prior to commencement of infrastructure and servicing works in the relevant sites, reports confirming that each completed stage of the remediation works for the contaminated sites meet the Department of Environment Regulation guidelines being provided to the City of Perth.***
4. ***In respect of Condition 6, the approved Construction Management and Staging Plan for the Elizabeth Quay project is to address, but not be limited to:***
 - a) ***vehicle, cycle and pedestrian access and movement through and/or around the project area;***
 - b) ***delivery of materials and equipment to and from the site;***
 - c) ***storage of materials, equipment and machinery on the site;***
 - d) ***parking arrangement for contractors and subcontractors;***
 - e) ***dewatering, including filtration and settlement details including measures to the impacts on surrounding sites including the Perth Convention and Exhibition Centre;***
 - f) ***a Noise Management Plan for the proposed works being submitted and approved by the City;***
 - g) ***delivery, stock piling and transport of clean fill to and from the site;***
 - h) ***onsite treatment, stock piling and removal of excavated materials including truck routes, queuing and parking;***

-
- i) any environmental matters identified in the environment assessment reports; and*
 - j) any other matters likely to impact on the surrounding properties and public areas.*
 - 5. In respect of Condition 7, the Management Plan for the rock revetment infrastructure is to address, inter alia, the structural integrity and management of the revetment.*
 - 6. The Utility Services Management Plan required under Condition 12 shall address, but not be limited to, protection and maintenance requirements (including appropriate buffers) for:*
 - a) the Cathode facility;*
 - b) the Wastewater Pump Station Overflow;*
 - c) maintenance vehicle parking and any Access Chambers modifications for the Main Drain.*
 - 7. The Urban Water Management Plan required under Condition 13 is to include, inter alia, a management plan for the bio-retention system. This management plan is to incorporate an extended monitoring program to ensure the long term effectiveness of the system and is to include not just the base of the submerged zone but also the outlet of the system.*
 - 8. The applicant is advised to contact the Department of Indigenous Affairs to ensure that the proposed works do not breach any section of Part IV (Protection of Indigenous Sites) of the Aboriginal Heritage Act 1972.*
 - 9. The Swan River Trust and City of Perth advise the applicant that the proposal has the potential to be affected by sea level rise. It is recommended applicants understand the implications of potential sea level rise on their site. The onus rests with the applicant to undertake a risk assessment and exercise their judgment in determining the level of risk they are prepared to accept.*

The Swan River Trust's Climate Change Risk Assessment Project (2010) models impact of potential sea level rise in the Swan and Canning Rivers, and sets out a methodology to assess the vulnerability of foreshore areas. While mapping information is available from the Trust, it is also recommended applicants undertake their own research and obtain appropriate independent professional advice relevant to the particular circumstances.

In the above regard, prior to works commencing in relation to the infrastructure and servicing for the public realm the applicant is to ensure that the proposal demonstrates compliance with the Western Australian Planning Commission's State Planning Policy 2.6 'State Coastal Planning Policy' in respect of the following matters:

- a) sea level rise leading to permanent and/or transient inundation;*

-
- b) *increased intensity of extreme events leading to storm inundation or sea level inundation;*
- c) *riverbed erosion due to a range of climate variables; and*
- d) *building subsidence due to a range of climate variables.*
10. *The applicant is advised that it is an offence to pollute or allow pollution to be caused to the river under Part V of the Environmental Protection Act 1986.*
11. *The applicant is advised that all piped and wired services (including fire hydrants, booster cabinets, service cabinets, transformers and power sources) and all other service related hardware will be required to be concealed from public view and located to minimise the impact on the streetscape and public realm, details of which will be required to be provided as part of future development applications for public realm and landscaping works in Elizabeth Quay.*
12. *In respect of Condition 17, the pedestrian/cycle bridge structure is to be constructed to Australian and Main Roads Western Australia Standards in accordance with the specifications highlighted for conflict between cyclists and pedestrians, gradients and sight lines, bridge cross-section, bridge decking and finishes and maintenance.*
13. *In respect of Conditions 18 and 19, the Department of Transport advises the applicant that prior to delivery of the relevant components of the development approval a working group is to be set up to address safety and operational issues relating to the movement of cyclists and pedestrians within the site, focusing on the provision of cycling facilities, shared spaces and the proposed cyclist/pedestrian bridge. The working group is to include the City of Perth, Metropolitan Redevelopment Authority, Department of Transport and Main Roads Western Australia and is to be engaged through the construction, implementation and opening stages. The Working Group will be limited to reviewing safety and operational matters but will not be reviewing the design elements.*

SUMMARY:

The key points relating to this report are as follows:

- The subject development application (DA3) proposes works associated with the public realm and includes hard and soft landscaping finishes including installation of a water feature, construction of the Landing and finishes to Riverside Drive, Howard Street and Sherwood Court extensions, construction and landscaping of the Island, construction of Barrack Square and associated hard and soft landscaping, installation of street furniture and heritage interpretation, lighting and signage/way finding, construction of short term parking, construction of the kiosk, ferry canopy and pedestrian bridge and the installation of public art as well

as service and infrastructure delivery and navigational aids and courtesy moorings.

- The proposal is generally consistent with the Masterplan for the Perth Waterfront, which was released by State Government in February 2011 and that was prepared by the Department of Planning under the oversight of the Perth Waterfront Ministerial Taskforce.
- The works the subject of this application are partially within and adjacent to the Swan River Trust's (SRT) Development Control Area for the Swan River. Accordingly, the works proposed require determination by the WAPC in a manner consistent with the advice of the SRT (pursuant to Clause 30(A)2(a) of the Metropolitan Region Scheme (MRS). The SRT supports the proposed works subject to conditions.
- The application is recommended for conditional approval

BACKGROUND:

Perth Waterfront Masterplan

In February 2011 the State Government released the final Masterplan for the Perth Waterfront. The Perth Waterfront Masterplan covers an area of approximately 10ha framed by William Street to the west, The Esplanade to the north, Barrack Street and Barrack Square to the east, and the Swan River to the south.

The key elements of the Masterplan include:

- A new urban waterfront precinct to be built as a seamless extension of the city to the river.
- A new inlet to bring the river closer to the foot of the city, to be framed by uninterrupted public terraces, promenades and civic spaces.
- An island within the inlet intended to provide a unique landscaped experience, and a range of recreational, interpretive and public event opportunities. The island is to be connected by two bridges to the inlet promenades to complete a pedestrian circuit around the waterfront.
- A nationally significant centre for indigenous culture, art and learning that is to be a major centrepiece for the new waterfront.
- Buildings that will reflect the scale of the city, be of a high architectural quality, and which will provide an active mix of residential, commercial, office, retail, hotel and hospitality uses.
- Strengthening of William and Barrack Streets as important connections through the city from the river to Northbridge.
- The extension of Howard Street and Sherwood Court to the water's edge, to enhance their role as activated pedestrian connections into the heart of the city.
- The diversion of Riverside Drive between Barrack and William Streets and the redistribution of major traffic to The Esplanade and other regional routes.
- The construction of a new, tree-lined, shared use, low speed waterfront road between Barrack and William Streets to enhance the recreational drive along the Swan River.
- Relocation of the commuter ferry terminal to the new inlet, to allow passengers to embark and disembark closer to the city and so as to create a stronger connection with the Esplanade Train Station and Bus Port.

-
- Enhancement of Barrack Square to integrate it as part of the new waterfront precinct, and so as to provide greater context to the Bell Tower.
 - Enhancement of Supreme Court Gardens as a major outdoor event and ceremonial space.
 - Re-establishment of the city's connection with King's Park via a cable car from the end of William Street.

LEGISLATION / STRATEGIC PLAN / POLICY:

Legislation

Planning and Development Act 2005

Metropolitan Region Scheme

Improvement Plan No. 35

Swan and Canning Rivers Management Act 2006

Section:

Planning and Development Act - Sections 5, 6 and 119

MRS - Clauses 16 and 30

Strategic Plan

Strategic Goal:

Planning

Outcomes:

Effective Delivery of Integrated Plans

Planned Local Communities developing a sense of place

Strategies:

Implement State and Regional Planning priorities

Encourage innovation in the design of our communities

Identify key Infrastructure and Development Projects

Policy

Number and / or Name:

Directions 2031; SPP 3 - Urban Growth and Settlements

DETAILS:

This report is presented to the SPC for its determination as the application sits within the Perth Waterfront Project area as this application was lodged with the WAPC prior to the transferring of the Waterfront area to the Metropolitan Redevelopment Authority (MRA). Given a previous agreement between the MRA and the WAPC, the WAPC is the determining authority for this application.

Earlier Development Applications for the Perth Waterfront have been determined by the Central Perth Planning Committee (CPPC), however the role of the CPPC has been reviewed by the WAPC and it is only considering more strategic matters and as such the application is presented to the SPC for consideration.

The subject land is reserved Public Purpose – Special Use and Waterways under the MRS. The City of Perth's City Planning Scheme No.2 (CPS 2) reflects this higher order MRS reservation (**Attachment 1: Locality Plan**). The Public Purpose - Special Use reservation was passed through the Upper House of Parliament on 6 March 2012, and serves to consolidate the existing reserves across the project area.

Clause 30 of the MRS requires the WAPC to have regard to the following factors when determining a development application:

- (i) the purpose for which the land is zoned or reserved under the MRS;
- (ii) the orderly and proper planning of the locality; and
- (iii) the preservation of the amenities of the locality.

Elizabeth Quay (**Attachment 2 Elizabeth Quay Masterplan Area**) proposes an extremely high quality public domain, which will raise the bar for the public realm experience within the city, yet ties in with the broader CBD and the materials which characterise it. With the exception of short term plans for Barrack Square and improvement works to Supreme Court Gardens, all remaining aspects of the public domain and associated servicing infrastructure are to be constructed as a part of this development application. The works proposed as part of this application include (**Attachment 3 Illustrative Site Plan**):

FEATURE	DESCRIPTION
<p>The Promenade and edge</p>	<p>The promenade is a split level walkway which surrounds the three terrestrial sides of the newly created inlet. In general, the combined upper and lower promenades will result in a minimum width of 20.5m. Public access between the upper and lower level is granted via stairs, inclined walkways and seating terraces. The upper level, approximately 2.5m above mean water level, provides a generous amount of space for alfresco dining and promenading. A strong definitive 'ribbon of trees' flanks the upper terrace and marks the point of transition to the lower promenade below.</p> <div style="display: flex; justify-content: space-around;">   </div> <p style="text-align: center; font-size: small;">VIEW OF PROMENADE LOOKING SOUTH</p>
<p>Station Park and Water Feature</p>	<p>Key gateway to EQ, located opposite the Esplanade train station including</p> <ul style="list-style-type: none"> - To have seating, a water feature, and alfresco areas - To be framed by Moreton Bay figs along William St, tall palms (relocated from the project area), understorey planting to frame the water feature (a wet/dry lake) - Will have bio-retention swale on western side for surface water runoff <p>An image of the water features is provided below:</p>

	
<p>The Landing</p>	<p>At the mid-point of the new Riverside Dr, intended as a destination point and including:</p> <ul style="list-style-type: none"> - Timber steps hovering about the high tides water level, with seating towards river - During functions, will extend to 55m to incorporate new Riverside Dr and allow all pedestrian crossings
<p>New Riverside Drive and other streets and parking</p>	<p>The new Riverside Drive connects Barrack and William Street, along the northern edge of the inlet. Constructed as a lower speed, shared use zone, the road and verges will share the high quality paving treatment of the promenade to ensure the continuation of the pedestrian connection from the city to the water's edge. The application also proposes the continuation of the city grid via the extension of Howard Street and Sherwood Court.</p> <p>The application includes the provision of on-street parking, the location of a new CAT service and the inclusion of taxi rank facilities along this road which will contribute to the amenity of the precinct, while also acting as traffic slowing devices to curtail fast through movement. The application also proposes street tree planting and paving treatments.</p> <p>On-street car parking will be constructed as a result of the works proposed in this application and will be a mix of embayed and non-embayed parking depending on the location and requirements. Although the length of the road network will increase within the Elizabeth Quay precinct, the existing level of on-street parking will be maintained. Provision of short-term, on-street paid parking will add to the dynamic of roads and provide for a range of uses including service vehicles, ACROD parking and general short-stay. A total of 144 on-street bays will be provided for use by a range of users including taxis, visitors, service vehicles etc.</p>
<p>William St Landing</p>	<ul style="list-style-type: none"> - Temporary, high quality treatment (until Indigenous Centre proceeds) - Ramp for bridge to be connected to the Promenade by series of

	<p>terraces including</p> <ul style="list-style-type: none"> - Materials and finishes to blend with that of the Promenade works - To accommodate ramp structures for the pedestrian/cycle bridge to the Island (to be in the SW corner of site 2).
<p>The Island</p>	<p>The island is proposed to be predominantly a passive garden space and a destination accessible by the pedestrian bridge to the west and a bridge allowing service vehicle access. The island will incorporate a series of winding paths, landscaping and retaining walls</p> <p>.</p> <p>The applicants advise that the Island will be an accessible public landscape providing a rich botanic experience. Mature trees give the Island immediate impact; not only within the island itself, but visually from the promenade the canopies will provide a generous volume of green amenity for the surrounding development.</p> <p>On the north-eastern corner of the island, a loose shell-grit beach will allow access onto the island by black swans and kayakers, as well as allowing engagement with the water by island patrons. This location is protected from the wind and wave action, and the water is relatively shallow providing good conditions for swans. An image of the proposed island is provided below:</p>  <p>The image is an aerial architectural rendering of a landscaped island. The island is irregularly shaped and surrounded by blue water. It features a dense canopy of green trees, winding white paths, and a wooden bridge connecting it to the mainland on the left. In the background, a marina with several boats is visible. The overall scene is bright and clear, suggesting a sunny day.</p>
<p>Barrack and Riverside passages</p>	<p>Important pedestrian connection between Barrack Square and the Promenade including:</p> <ul style="list-style-type: none"> - Location of functional requirements of jetty and waterfront (fuel and sullage facilities, and temporary transformer) - Significant grade change for flood and climate change requirements (via stair access and inclined paved walkway)
<p>Barrack Square (long term)</p>	<p>High quality public space with play space in middle, central linear water feature, and Bell Tower including:</p> <ul style="list-style-type: none"> - Timber plaza in western half, retention of trees and plantation in

treatment)	<p>Paradise style on eastern side</p> <ul style="list-style-type: none"> - Central pedestrian walkway connecting Supreme Gardens, circular vehicular circulation, alternative surface treatment to the south to indicate shared space, continuation of shared path - Short term Barrack Square proposal part of separate DA given approval by WAPC; long term proposal subject to detailed design refinement.
Heritage interpretation	Proposed through signage, artefact display, artworks, interpretation (including audio and digital), events.
Street furniture	Includes seats, bins, drinking fountains, signage, pole and traffic lights, tree grates, balustrades, handrails.
Lighting	Lighting strategy to be coherent and sustainable.
Signage and wayfinding	e.g. major destinations outside of EQ like public transport, retail, entertainment destinations.
Movement network (including parking)	Short term parking to be provided by way of providing 144 parking bays for use by multi users.
Kiosks	3 proposed as part of this DA (2 on western flank and 1 on eastern) and to represent 'buoys', to be used for information purposes, a small retail component, ferry services, toilets and to be constructed within the public realm areas.
Ferry canopy	The ferry jetty and associated canopy are to be located on the north-west corner of the inlet. Providing weather protection for passengers, the canopy extends over both the pontoon and the ramp, as well as partially over the promenade for ticketing and passenger waiting areas. As an architectural feature, the canopy will comprise of contoured structural fins and fritted patterned glass, reinforcing the curvilinear pattern of the promenade.
Pedestrian bridge	<p>Based on a bracelet design concept, the pedestrian bridge will be constructed to allow for shared pedestrian and cyclist use, and incorporate an operable portion allowing for special event access into the inlet for larger boats.</p> <p>The bridge projects away from the shore into the Swan River – evoking a launching experience for the user and creating expanding views back towards the city. A singular ramp extends from the western mainland, rising to an operational cog pivoting mechanism. This pivot armature allows disconnection from the primary bracelet to facilitate an opening for larger recreational craft for specific events. Another connection ramp leads the pedestrian from the floating bracelet to the new eastern island landing.</p>

Public art	A Public Art Strategy has been prepared and endorsed by the MRA, setting out the objectives, key locations and delivery strategy for public art within the precinct. In addition to public art provision within private sector development, three key nodes have been identified within the strategy for more considerable artist involvement and include the Pedestrian Bridge, the Edge and Station Park.
Navigational aids	<p>New navigation aids or markers will be provided in the inlet and river to delineate safe navigation for vessels. The new navigation markers will be placed at the edge of the dredged access channels and be consistent with existing markers in Perth Water and will include::</p> <ul style="list-style-type: none"> • 2 new port markers to mark new access channel into inlet; • starboard marker inside the inlet to mark edge of dredged area; • south cardinal marker south of island to mark safe navigable water; • - special mark for extent of access channel through operable section of bridge (only for event use); and • removal of existing port marker south of the proposed island location. <p>The Swan River Trust (SRT) currently manages two courtesy moorings to the west of the proposed development, in Perth Water. It is proposed to add 10 additional courtesy moorings in this location, primarily for use by larger vessels that may be unable to enter the inlet (except when the operative bridge is in effect).</p>
Courtesy moorings	SRT has 2 existing courtesy moorings west of the proposed development, in Perth Water. The application proposes 10 additional courtesy moorings proposed in this location for larger vehicles that cannot enter the inlet (other than when bridge is operational). Location and form currently being developed in accordance with DOT standards.
Services and Infrastructure	<p>Two transformers serving the public realm will ultimately be sited within the built form on private development lots, to reduce the visual impact on the public domain. In the interim, discrete areas of the public realm or development sites with a longer construction timeframe are currently being considered until such time as the end location is constructed.</p> <p>Water supply mains are to be extended to service the development and will require the following:</p> <ul style="list-style-type: none"> - Construction of a new 800 diameter distribution main being constructed to the existing 900 diameter main at the corner of Barrack Street and New Riverside Drive - Relocation of the existing 400 diameter water main running from The Esplanade to the Sherwood Court extension

	<ul style="list-style-type: none"> - Installation of a 250 diameter water main along William Street and north of Sites 2 and 3 stopping at the New Riverside Drive road reserve. - Installation of a 250 diameter water main along New Riverside Drive, along with a 250 pipe along Barrack Street to Barrack Square and a new 200 diameter main around Barrack Square past Site 10.
Drainage and Water Sensitive Urban Design	New stormwater drainage infrastructure will be installed to new roads, including Barrack Square. Water sensitive urban design initiatives such as tree pits and soakage cells are proposed to slow the water flow and provide treatment prior to disposal/infiltration to the aquifer.

GOVERNMENT AND CORPORATE IMPLICATIONS:

Improvement Plan No.35

Improvement Plan No.35 was established for the Perth Waterfront in November 2010 under Part 8 of the Planning and Development Act 2005. The Improvement Plan confers on the WAPC the authority to plan, design, consolidate, subdivide, clear, develop or make necessary changes to land held by it; or enter into agreement for such purposes with any owner of land not held by it within the Improvement Plan area.

Metropolitan Redevelopment Scheme

The WAPC is the accountable authority for the Perth Waterfront until such time as the Metropolitan Redevelopment Authority (MRA) has prepared a Redevelopment Scheme for the Central Perth area, and which includes the Perth Waterfront project area. MRA. In late 2012, the Minister for Planning approved the gazettal of an amendment to the Central Perth Redevelopment Scheme, officially transferring statutory planning authority for the Elizabeth Quay project area from the WAPC to MRA. The amendment to the Redevelopment Scheme also introduces provisions into the scheme relevant to Elizabeth Quay.

CONSULTATION:

The City of Perth, MRA, Heritage Council of WA, Department of Water, Department of Environment and Conservation, SRT, the Department of Transport (on behalf of the Transport Portfolio) and Western Power have assessed the proposed application (inclusive of addendum) and recommend approval subject to conditions.

City of Perth Comments

The City has recommended approval of the development application subject to conditions, the majority of which have been included in the recommendation. Conditions that were considered not to fall within the scope of the application, have not been included.

Transport Portfolio Comments

The Transport Portfolio advises of its support of the application subject to conditions. All conditions recommended by the Department of Transport have been included in the recommendation.

Swan River Trust Comments

The Trust has considered the application and has resolved to recommend that the application be approved subject to conditions. The Trust has however raised concerns regarding the proposed hard edge treatment to The Island and has advised that it would prefer to see the use of a more natural treatment featuring vegetation. The recommendation however supports the use of hard landscaping.

Most significantly, the Trust does not support the provision of 10 additional courtesy moorings. This concern has been addressed in the recommended conditions, which includes a condition to state that the moorings do not form part of this approval.

OFFICER'S COMMENTS:

The subject application proposes works associated with the public realm and includes hard and soft landscaping finishes, which will be focussed on an extremely high quality public domain. The application includes the installation of a water feature, construction of the Landing and finishes to Riverside Drive, Howard Street and Sherwood Court extensions, construction and landscaping of the Island, construction of Barrack Square and associated hard and soft landscaping including the installation of street furniture and heritage interpretation, lighting and signage/way finding, construction of short term parking, construction of the kiosk, ferry canopy and pedestrian bridge and the installation of public art as well as service and infrastructure delivery and navigational aids and courtesy moorings.

The proposal is generally consistent with the Masterplan for the Perth Waterfront, which was released by State Government in February 2011 and that was prepared by the Department of Planning under the oversight of the Perth Waterfront Ministerial Taskforce.

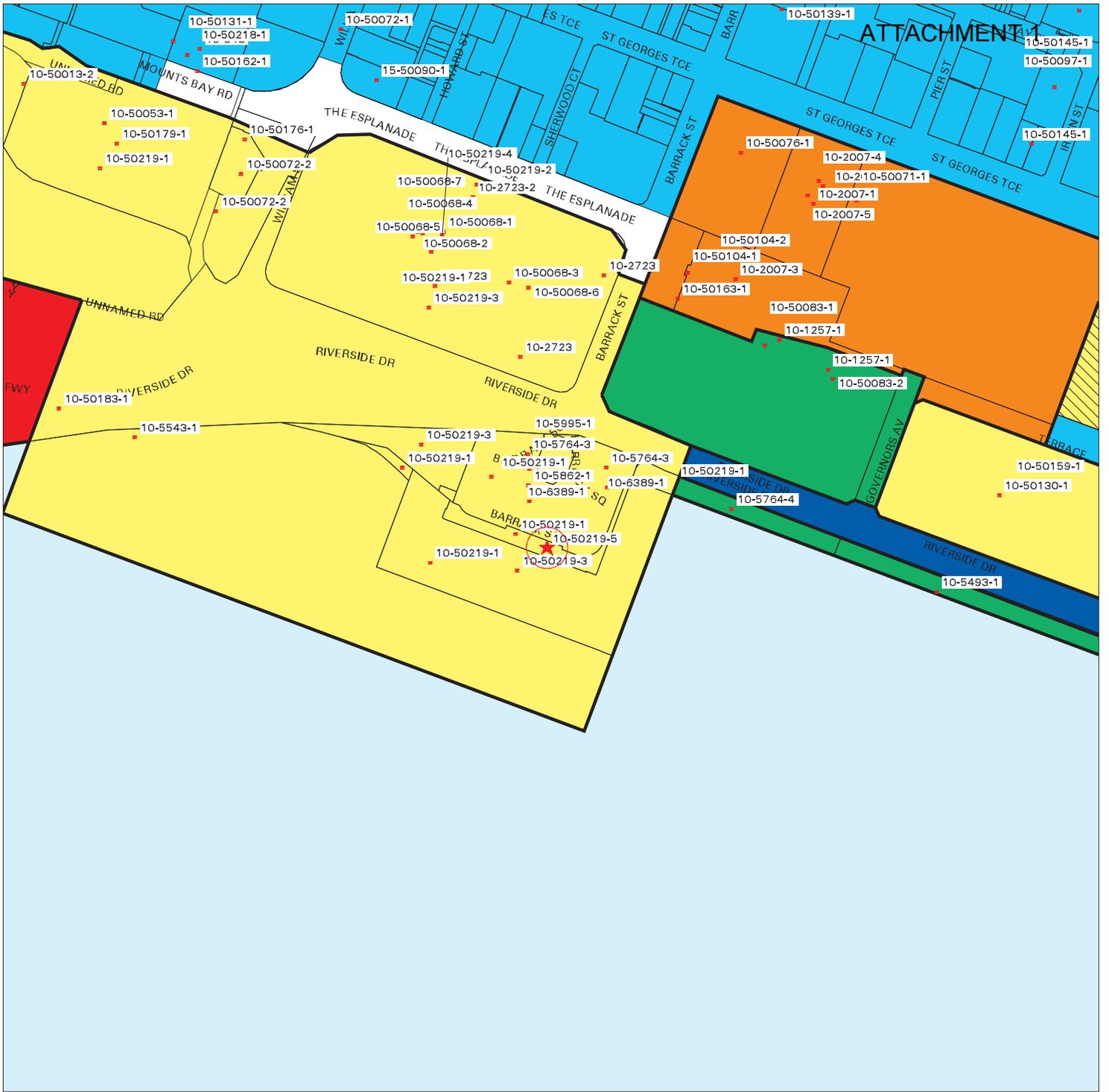
Concerns that have been raised by servicing agencies have been addressed through the imposition of conditions, which are included in the recommendation.

CONCLUSION

The proposed inlet construction and associated works the subject of this application will facilitate implementation of the focal point of the Perth Waterfront project. The proposal complies with the intent of the reservation of the MRS and with the higher order planning that has occurred for the project area, and represents orderly and

proper planning. The development will improve the amenity of the subject land and facilitate greater use of that land.

Given the above, it is recommended that the Statutory Planning Committee support the application subject to conditions.



Development Application 10-50219-5 (MGA ref 392092mE 6463460mN Zone 50)

This data is to be used for the processing of subdivision applications only.

-  APPROX LOCATION OF APPLICATION
-  PRIMARY REGIONAL ROADS
-  OTHER REGIONAL ROADS
-  WATERWAYS
-  PARKS & RECREATION
-  CIVIC & CULTURAL
-  PP - CAR PARK
-  CADASTRAL BOUNDARY
-  WESTNET ENERGY GAS PIPELINE
-  DEVELOPMENT APPLICATION
-  CITY CENTRE
-  PUBLIC PURPOSES CIVIC USE



Attachment 2 - Elizabeth Quay Masterplan Area

NOTES
ALL DIMENSIONS ARE IN MILLIMETERS. DO NOT SCALE DRAWINGS.
ENSURE THIS DRAWING HAS BEEN PRINTED TO SCALE BY CHECKING
MEASURING BAR BOTTOM LEFT CORNER. VERIFY ALL DIMENSIONS ON
SITE BEFORE COMMENCING ANY WORK OR SHOP DRAWINGS OR
ORDERING ANY MATERIALS.



AMENDMENTS SINCE DRAWING CREATED INDICATED.

AMENDMENT NOTES FOR THIS ISSUE

Rev.	REASON FOR THIS ISSUE:	DATE
07/06/12	GENERAL ISSUE	

NOTES

ARM
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Taylor Cullity Lethlean
Landscape Architecture
Urban Design
Community Consultation

PROJECT
PERTH WATERFRONT

CLIENT
METROPOLITAN REDEVELOPMENT AUTHORITY
GOVERNMENT OF WESTERN AUSTRALIA

TITLE
ILLUSTRATIVE SITE PLAN

DATE	07/06/12	FOR INFORMATION
DRAWN	ARM/TCL	
T. LEADER	SA	JOB NUMBER 0657
SCALE	A1	DRAWING NO. PWP3-TCL-LS-DWG 1024

ITEM NO: 9.3

Four Multiple Dwellings - Lot 1 (No. 12) Cobham Avenue, Nollamara

WAPC OR COMMITTEE: Statutory Planning Committee

REPORTING AGENCY: Department of Planning
REPORTING OFFICER: Senior Planning Officer, Metropolitan Central
AUTHORISING OFFICER: Director, Metropolitan Central
AGENDA PART: G
FILE NO: 20-50459-1
DATE: 12 February 2014
ATTACHMENT(S): 1. Development Plans
2. Location and Zoning Plan
3. Aerial Photo
REGION SCHEME ZONING: MRS: Urban
LOCAL GOVERNMENT: City of Stirling
LOCAL SCHEME ZONING: Residential R40
LGA RECOMMENDATION(S): Refusal
REGION DESCRIPTOR: Perth Metro Central
RECEIPT DATE: 31 May 2013
PROCESS DAYS: 258
APPLICATION TYPE: Development
CADASTRAL REFERENCE: Lot 1 Cobham Avenue, Nollamara

RECOMMENDATION:

That the Western Australian Planning Commission resolves to approve the proposed development of four multiple dwellings at Lot 1 (No. 12) Cobham Avenue, Nollamara, subject to the following conditions:

- 1. The development shall be carried out only in accordance with the terms of the application as approved herein, and the attached plans (Unit 1&2 Layout 1 and 2, Unit 1&2 Elevations) date stamped 30 May 2013, and attached plans (Site Plan, Unit 3&4 Layout 1 and 2, Unit 3&4 Elevations) date-stamped 6 January 2014, and by the Department of Planning on behalf of the Western Australian Planning Commission.***
- 2. Prior to the issue of a building permit, the plans are to be amended to show:***

-
- (i) *a maximum internal driveway taper of 1:5 on the south-western side of Unit's 2's driveway; and*
 - (ii) *the proposed crossover wings for the two proposed crossovers are to comply with the City of Stirling's Engineering Department specification requiring 1.8m x 1.8m crossover wings; to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
 3. *Gradients to driveways and ramps serving parking areas are to comply with Australian Standard 2890.1, and no changes to verge levels are permitted, to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
 4. *All driveways, parking, manoeuvring and circulation areas are to be constructed of brick paving (or alternative finishes such as concrete or bitumen are acceptable if it has a decorative type finish) and sealed, drained and maintained to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
 5. *Vehicular parking, manoeuvring and circulation areas indicated on the approved plan being sealed and drained, the six (6) parking spaces being marked out and maintained in good repair to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
 6. *One (1) visitor bicycle space is to be provided in a visible location within the front setback areas on site to the specification the local government and the satisfaction of the Western Australian Planning Commission.*
 7. *Any existing crossovers not included as part of the proposed development on the approved plan are to be removed and new kerbing and verge to be reinstated, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
 8. *Any on-site clothes drying facilities being screened from public view, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
 9. *Pedestrian pathways providing wheelchair accessibility connecting all entries to buildings with a public footpath and car parking areas to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
 10. *Adequate lighting being provided to communal pathways and parking areas, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
 11. *No walls, fences or letterboxes above 0.75 metres in height to be constructed within 1.5 metres of where:*
-

-
- (i) walls or fences adjoin vehicular access points to the site; or*
 - (ii) a driveway meets a public street; or*
 - (iii) two streets intersect;*

unless the further approval of Council is obtained, to the specification of the local government and the satisfaction Western Australian Planning Commission.

- 12. All land indicated as landscaped area on the approved plan being developed on practical completion of the building/s, and all landscaped areas maintained in good condition thereafter, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
- 13. All eaves to the proposed development to maintain a minimum setback of 750mm from the boundary, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
- 14. Stormwater from all roofed and paved areas to be collected and contained on site, to the specification of the local government and the satisfaction Western Australian Planning Commission.*

ADVICE TO APPLICANT

- 1. Where an approval has lapsed, no development shall be carried out without further approval having been first sought and obtained.*
- 2. The vehicular access shown in this application has been assessed and determined based upon the location of street trees as shown on the submitted plans. It is the responsibility of the applicant to ensure that this information is correct as any inaccuracy of the plans will not be considered justification for removal of the trees in the event that their positions are incorrectly shown. Removal of street trees without the written approval of the City of Stirling is an offence.*
- 3. This is not a building licence or an approval to commence or carry out development under any other law. It is the responsibility of the applicant to obtain any other necessary approvals, consents and licences required under any other law, and to commence and carry out development in accordance with all relevant laws.*
- 4. This approval is not an authority to ignore any constraint to development on the land, which may exist through statute, regulation, contract or on title, such as an easement or restrictive covenant. It is the responsibility of the applicant to investigate any such constraints before commencing development. This approval will not necessarily have regard to any such constraint to development.*

-
5. **Noisy Construction Work outside the period 7:00am to 7:00pm Monday to Saturday and at any time on Sundays and Public Holidays is not permitted unless an approved Noise Management Plan for the construction site has been issued.**
 6. **The street address for the development will be unit numbers: 1/12 (Unit 1), 2/12 (Unit 3), 3/12 (Unit 2) and 4/12 (Unit 4) Cobham Avenue, Nollamara.**
 7. **All street trees located in verge areas adjoining the subject lots are to be retained and protected.**
 8. **All boundary fencing behind the front setback line is to accord with the provisions of the City's Local Laws pertaining to the provision of a sufficient fence.**

SUMMARY:

The proposal is for four multiple dwellings, to be constructed by the Department of Housing (DOH). The application is required to be determined by the Western Australian Planning Commission (WAPC) as the recommendation is inconsistent with the recommendation of the local government and the proposal is not consistent with the deemed to comply provision of the Residential Design Codes (R-Codes) with regard to street setback, side and rear setback, parking, vehicle access (crossovers) and storerooms.

LEGISLATION / STRATEGIC PLAN / POLICY:

Legislation *Planning and Development Act 2005 - Part 10 - Development Approval*
Section: Section 162

Strategic Plan
Strategic Goal: Planning
Outcomes: Effective Delivery of Integrated Plans
Strategies: Implement State and Regional Planning Priorities

Policy
Number and / or Name: State Planning Policy 3.1 Residential Design Codes

INTRODUCTION:

The subject application submitted by the DOH seeks approval to construct four multiple dwellings on a 825m² lot (**Attachment 1 - Development Plans**). The proposed development is estimated to cost \$920,000 and the estimated time of completion is one year.

The site is zoned 'Residential' with an R40 density coding under the City of Stirling Local Planning Scheme No. 3 (LPS3) and 'Urban' under the Metropolitan Region Scheme (MRS) (**Attachment 2 - Location and Zoning Plan**).

Pursuant to the provisions of the *Planning and Development Act 2005*, public works by public authorities do not require the approval of the local government under the planning scheme but require the approval of the WAPC under the MRS.

The WAPC has granted delegated authority for the DoH to determine its own applications under the MRS where such applications comply with the City's Town Planning Scheme/Codes/Policy and the proposed construction of no more than 10 dwellings of a height of two storey or less. The City of Stirling has advised that the proposed development does not comply and has referred the application to the WAPC for determination.

In accordance with the provisions of the MRS, in determining the merits of a development application, the WAPC is required to have regard to orderly and proper planning, the preservation of the amenities of the locality and the purpose of the land's zoning.

CONSULTATION:

The City's response dated 14 November 2013 recommends refusal of the development application for the following reasons:

- (i) Clause 6.1.1 of the Residential Design Codes (R-Codes) 'Building size' - Plot ratio is proposed to be 0.667 or 550.45m² in lieu of 0.6 or 495m². The bulk and scale is inconsistent with existing or future desired built form of the locality as the proposal has essentially overdeveloped the site.
- (ii) Clause 6.1.3 'Street setback' - The ground floor setback of Unit 1 is proposed to be 3.7m in lieu of 4.0m from the street.
- (iii) Clause 6.1.4 'Side and rear setbacks' - The Unit 4 living, kitchen, bath, ensuite and master suite is proposed to be setback 1.7m in lieu of 1.8m from the south-eastern boundary.
- (iv) Clause 6.3.3 'On-site parking provision' - Location B requires 6 car bays and 4 are provided. Location B requires 1 visitor bay and 1 bicycle bay and none are provided. The proposed development includes 4 two and three bedroom dwellings. The proposed car parking on site is insufficient as given the type and size of the dwellings it is reasonable to conclude that they are designed for families. There is no availability of on-street parking and no public transport within 250m of the site. The proposed parking is considered inadequate for the expected parking needs of the future occupants and their visitors.
- (v) Clause 6.3.4 'Design of parking spaces' - The Unit 3 single garage is proposed to be 2.9m in width in lieu of 3.0m in accordance with AS2890.1.
- (vi) Clause 6.3.5 'Vehicle Access' - Proposes two crossovers in lieu of one crossover permitted for 20m of street frontage that is visible from the street. The two crossovers are considered to detract from the streetscape and the proposal can easily be designed to accommodate just one crossover.
- (vii) Clause 6.4.1 'Visual Privacy' - The Unit 4 master suite is proposed to be setback from the south-western boundary by 4.25m in lieu of 4.5m. The Unit 4 balcony is proposed to be setback from the north-eastern boundary by 2.5m in

lieu of 7.5m. The proposed balcony and habitable window is considered a result of poor design and 'one size fits all' approach to development. The proposed incursion compromises the privacy of the adjoining property and is inconsistent with the level of privacy expected within the locality.

- (viii) Clause 6.4.6 'Utilities and Facilities' - The Unit 1 internal storage dimension is proposed to be 1.3m in width in lieu of 1.5m in width. The Unit 2 internal storage area is proposed to be 2.535m² in lieu of 4m², and the internal storage dimension is proposed to be 1.3m in lieu of 1.5m. The Unit 4 internal storage area is proposed to be 2.56m² in lieu of 4.0m².

The City has however provided recommended conditions should the WAPC be minded to approve the application.

COMMENTS:

While approval is limited to the MRS, the WAPC must still consult with the local government and give due regard to its local planning scheme and the principles of proper and orderly planning. However, the WAPC is primarily assessing this proposal under the MRS and not against LPS3. Notwithstanding this, and in acknowledgement of the City's objections, the following comments are made.

Building Size

The City of Stirling has included the car parking and garage areas in their calculation of plot ratio area. However, the definition of plot ratio area provided by the R-Codes specifically states that areas used exclusively for the parking of wheeled vehicles, storerooms and lobbies are excluded.

Calculating the plot ratio area using the definition provided by the R-Codes results in a total plot ratio area of 464.24m² which is less than the 0.6 ratio allowed (which would be 495m²). Therefore the proposed building size is acceptable.

Street Setback

The development proposes a setback of 3.7m from the primary street for Unit 1 in lieu of a required 4m as set out in the R-Codes.

The portion of building that is setback 3.7m is approximately 15% of the lot frontage. The rest of the building including the garages are setback 5.5m. The 0.3m variation is considered to be acceptable given the proposed dwellings will contribute to the desired streetscape and provide articulation of the building on the primary street.

Side and Rear Setbacks

The Unit 4 living, kitchen, bath, ensuite and master suite is proposed to be setback 1.7m in lieu of 1.8m from the south-eastern boundary. This variation is minimal and is considered acceptable.

Parking

The development provides six resident car parking bays, whilst the R-Codes require four resident bays and one visitor bay. Therefore, two additional resident bays are being provided, with no visitor bays.

Four resident bays in a tandem configuration for Units 1 and 2 are provided whilst the two rear units have one car bay each. None of the bays are for visitors.

Whilst the R-Codes requires the provision of 4 resident bays and 1 visitor bay, the low-speed residential environment of Cobham Avenue means that street parking for visitors would be appropriate and available (**Attachment 3 - Aerial Photo**).

Therefore it is considered that the variation of providing 6 resident bays in lieu of 4 bays and 1 visitor bay is acceptable.

A condition requiring the provision of one bicycle parking bay is recommended.

Design of Parking Spaces

Amended plans have been provided and the Unit 3 single garage is now denoted to be 3.0m wide in accordance with AS2890.1.

Vehicle Access

The application proposes two crossovers for a lot frontage of 20.5m in lieu of one crossover permitted for 20m of street frontage that is visible from the street.

However, there is a space of approximately 4m between the crossovers and a street tree will be located between them. There are several other grouped dwelling developments in the locality that also have two crossovers, and the existing development on this site also has two crossovers. Therefore it is considered that the proposed crossovers are acceptable and will not detract from the streetscape.

Visual Privacy

Amended plans have been provided which show the Unit 4 master suite window width reduced and the balcony relocated. The application now complies with Clause 6.4.1 'Visual Privacy' of the R-Codes.

Utilities and Facilities

The applicant has provided information that the Unit 1 and Unit 2 internal storage dimensions are 1.42m. While this falls short of the required 1.5m it is considered a minor variation is acceptable given the storage is in the garage and therefore the garage edges can also be utilised for storage purposes.

The applicant has also provided information that the Unit 4 storage area is 4.16m² and therefore complies.

Department of Housing Justification

The DOH has provided the following comments with regard to the proposed development:

- The Department's 'maisonette' units reflect contemporary housing options and are a move away from the conventional 'cookie cutter' style of infill development in the suburbs. It represents an innovative product that provides a broader choice of dwelling type and price points. The maisonettes are designed to maximise yield while blending with the surrounding properties as they 'present' as two-storey houses;
- It is anticipated that the subject proposal will continue to introduce two-storey residential dwellings into the Nollamara streetscape. This has been shown to encourage similar development over time, and will encourage a stronger mix of socio-economic groups in the community; and
- The proposed design aims to follow the objectives of the City's Balga-Westminster-Nollamara Local Area Plan, particularly with regards to introducing a greater range of housing types aimed at improving housing affordability.

CONCLUSION:

Although the proposed development is for multiple dwellings, comprising two-storey maisonette units with one dwelling per floor, the proposed buildings are not dissimilar to surrounding residential development, albeit two storeys. The proposal also provides housing variety.

In conclusion it is considered that the proposed residential scheme is an appropriate development for the 'Urban' zone under the MRS. The proposal satisfactorily meets the City of Stirling Local Planning Scheme No. 3, the requirements of the Residential Design Codes 2013 and represents proper and orderly planning of the locality. Further, approval of the proposed multiple dwellings will not adversely affect the amenity of the locality and will provide an alternative form of housing.

It is therefore recommended that this application be approved subject to conditions.



LOT 1
825sqm
SANDY

LOT 7074
BRICK & TILE
RESIDENCE
FLOOR RL 9.73

LOT 395
BRICK & TILE
RESIDENCE
WELL CLEAR

WARNING!
BOUNDARY RE-PEG SURVEY
REQUIRED TO CONFIRM PEG POSITIONS

NOTE: BOUNDARY POSITION AS PER ALIGNMENT REQUIRES REPEG SURVEY

NOTE: APPROXIMATELY 9 Km FROM COAST LINE

NOTE: ADD 65.54 TO ALL LEVELS ON PLAN TO OBTAIN APPROXIMATE A.H.D. LEVELS. THIS VALUE DERIVED FROM WATER CORP. AS CONSTRUCTED RECORDS & MAY NOT BE EXACT. AN A.H.D. SURVEY IS REQUIRED TO OBTAIN AN ACCURATE VALUE.

COASTAL CATEGORY 4

NOTE:
N1 WIND CLASSIFICATION. REFER TO ENGINEERS SPECIFICATION

NOTE:
STORMWATER DISPOSAL BY BUILDER TO COUNCIL REQUIREMENTS.

NOTE:
ALL RETAINING WALLS DO NOT ALLOW FOR EMBEDMENT. FOR EMBEDMENT, REFER TO ENGINEER'S DETAIL.

NOTE:
THE OWNER IS LIABLE FOR REMOVAL OF ANY BOUNDARY FENCES IF REQUIRED FOR CONSTRUCTION PURPOSES.

SITE COVERAGE CALCULATIONS U1 & U2

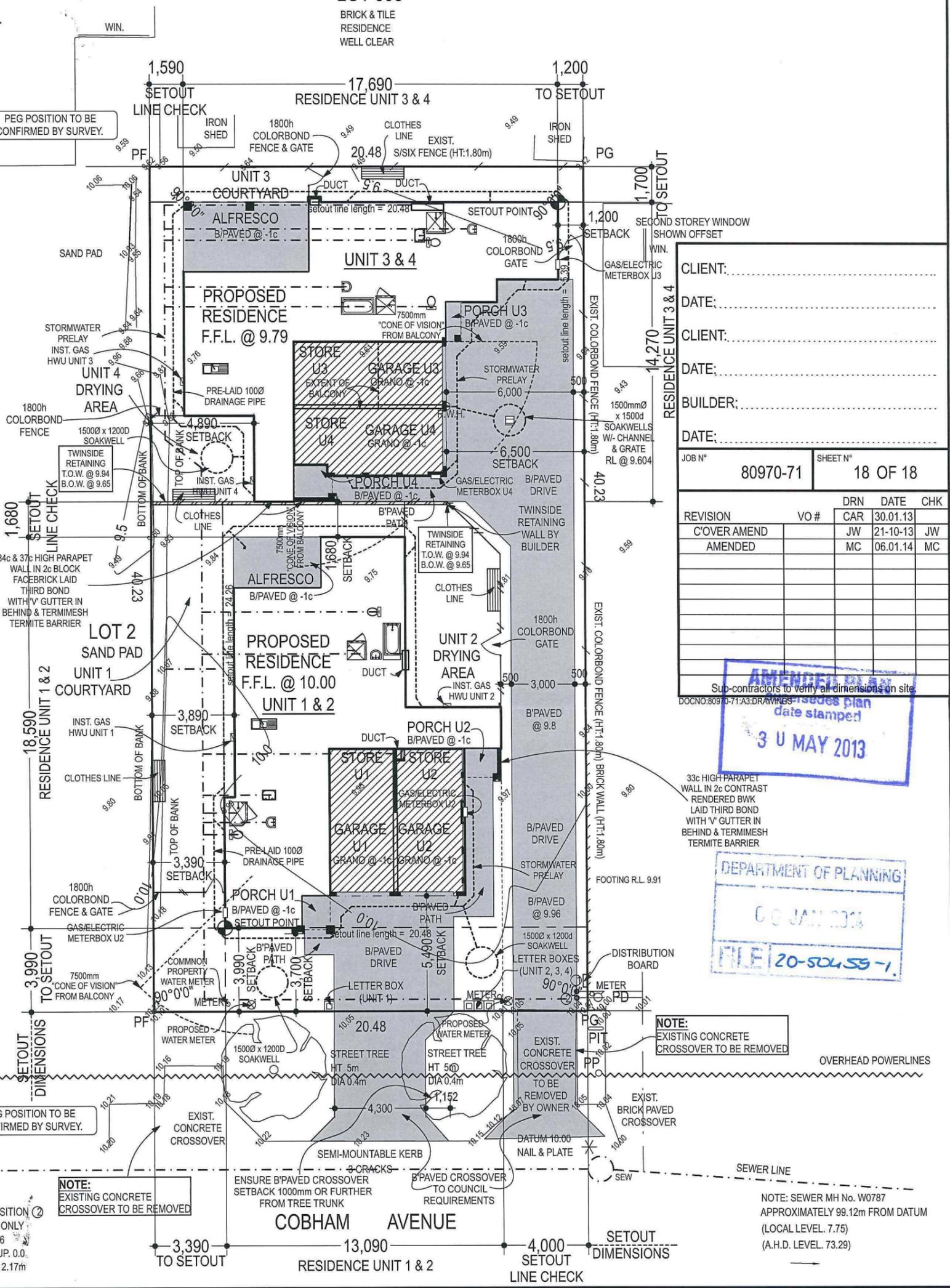
Total Site Area	396.68 m ²
House & Garage/Store Area	163.71 m ²
Site Cover (%)	41.27%

LOCATION	GRANO m ²	B'PAVED m ²
Alfresco	0.00 m ²	9.84 m ²
Garage, Porch, Store, Path & Drive	41.14 m ²	48.25 m ²
X'over	0.00 m ²	26.29 m ²

SITE COVERAGE CALCULATIONS U3 & U4

Total Site Area	428.32 m ²
House & Garage/Store Area	155.89 m ²
Site Cover (%)	36.40%

LOCATION	GRANO m ²	B'PAVED m ²
Alfresco	0.00 m ²	19.35 m ²
Garage, Porch, Store, Path & Drive	41.78 m ²	146.70 m ²
X'over	0.00 m ²	19.81 m ²



CLIENT:

DATE:

CLIENT:

DATE:

BUILDER:

DATE:

JOB N° 80970-71 SHEET N° 18 OF 18

REVISION	VO #	DRN	DATE	CHK
COVER AMEND		JW	21-10-13	JW
AMENDED		MC	06.01.14	MC

AMENDED PLAN
Sub-contractors to verify all dimensions on site.
DOCN0:80970-71-A3-DRAWINGS
date stamped
30 MAY 2013

DEPARTMENT OF PLANNING
06 JAN 2013
FILE 20-50459-1

NOTE:
EXISTING CONCRETE CROSSOVER TO BE REMOVED

NOTE: SEWER MH No. W0787 APPROXIMATELY 99.12m FROM DATUM (LOCAL LEVEL. 7.75) (A.H.D. LEVEL. 73.29)

CLIENT	DEPARTMENT OF HOUSING	BUILDER	IMPRESSIONS
LOT	1 COBHAM AVENUE	AUTHORITY	STIRLING
SUBURB	NOLLAMARA	MAP REF.	312 24 64
PLAN	25438	C/T Vol.Fol.	2176 / 828
DATE OF SURVEY	06.12.12	SCALE	1:200 @ A3
JOB No.	OURS: 182719	YOURS:	80771

SCALE BAR	DRAFTED	M.R.	SURVEYOR	J.A.
SERVICE INFORMATION		SEWERAGE	YES	DEPTH 2.18m/2.17m
GAS	BTC	WATER	YES	PRELAI
AREA	ESTABLISHED	TELSTRA	YES	POWER U/G / O/H BTC
WATER	SV STOP VALVE	HY HYDRANT	FP FLUSH POINT	METER WATER METER
SEWERAGE	SW SEWER MANHOLE	IS INSPECTION SHAFT	IQ INSPECTION OPENING	GAS GAS GAS METER
POWER	CP CONSUMER POLE	PP POWER POLE	LP LAMP POST	SP STAY POLE PD POWER DOME
STORMWATER	SW MANHOLE	GRATE	SIDE ENTRY PIT	TELSTRA TELSTRA PIT
SURVEY	PF PEG FOUND	Pdist PEG DISTURBED	PG PEG GONE	STATION DATUM / CONTROL

AUTOMATED SURVEYS
LICENSED SURVEYORS AND DEVELOPMENT CONSULTANTS

PTY LTD
3 Ord Street West Perth W.A. 6005
P.O. Box 1648 West Perth W.A. 6872
Telephone: +61 (08) 9214 1777
Facsimile: +61 (08) 9214 1778

The information on this drawing is current as at the date of survey. Services are to be confirmed with relevant Authorities. This includes without limitation: Sewerage, Water Supply, Drainage, Power Supply, Gas Supply & Communications. Further interests / notifications / encumbrances may be listed on the Certificate of Title. Location of boundary in relation to fences or boundary markers is not guaranteed. Boundary is placed arbitrarily as per best fit. Repeg Survey will be required. Copyright 2009 Automated Surveys Pty. Ltd. All rights reserved.



NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGMATICALLY ONLY.

(SA) DENOTES SMOKE ALARM

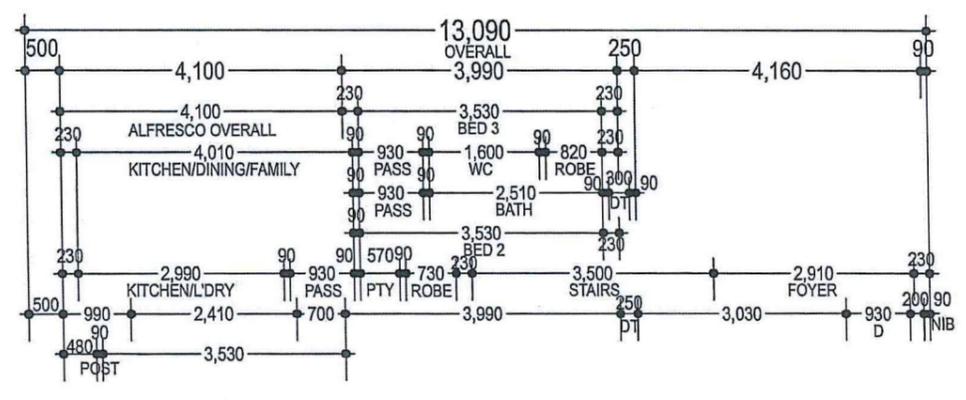
PLUMBING NOTE:

ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

NOTE:

- R2.0 INSULATION TO ALL EXTERNAL CAVITY WALLS.

AREAS: GROUND FLOOR		
	PERIM. (m)	AREA (m2)
HOUSE AREA (Ground)	63.36	119.68
GARAGE U2	18.08	19.23
GARAGE U1	17.24	16.86
ALFRESCO	13.00	9.84
STORE U2	9.34	4.56
STORE U1	8.78	4.17
PORCH U2	6.56	2.44
PORCH U1	8.03	2.39
		179.17 m2
GROUND ROOF AREAS		
Material Type	Area [m2 on the flat]	
Roof - Tiled	48.49	



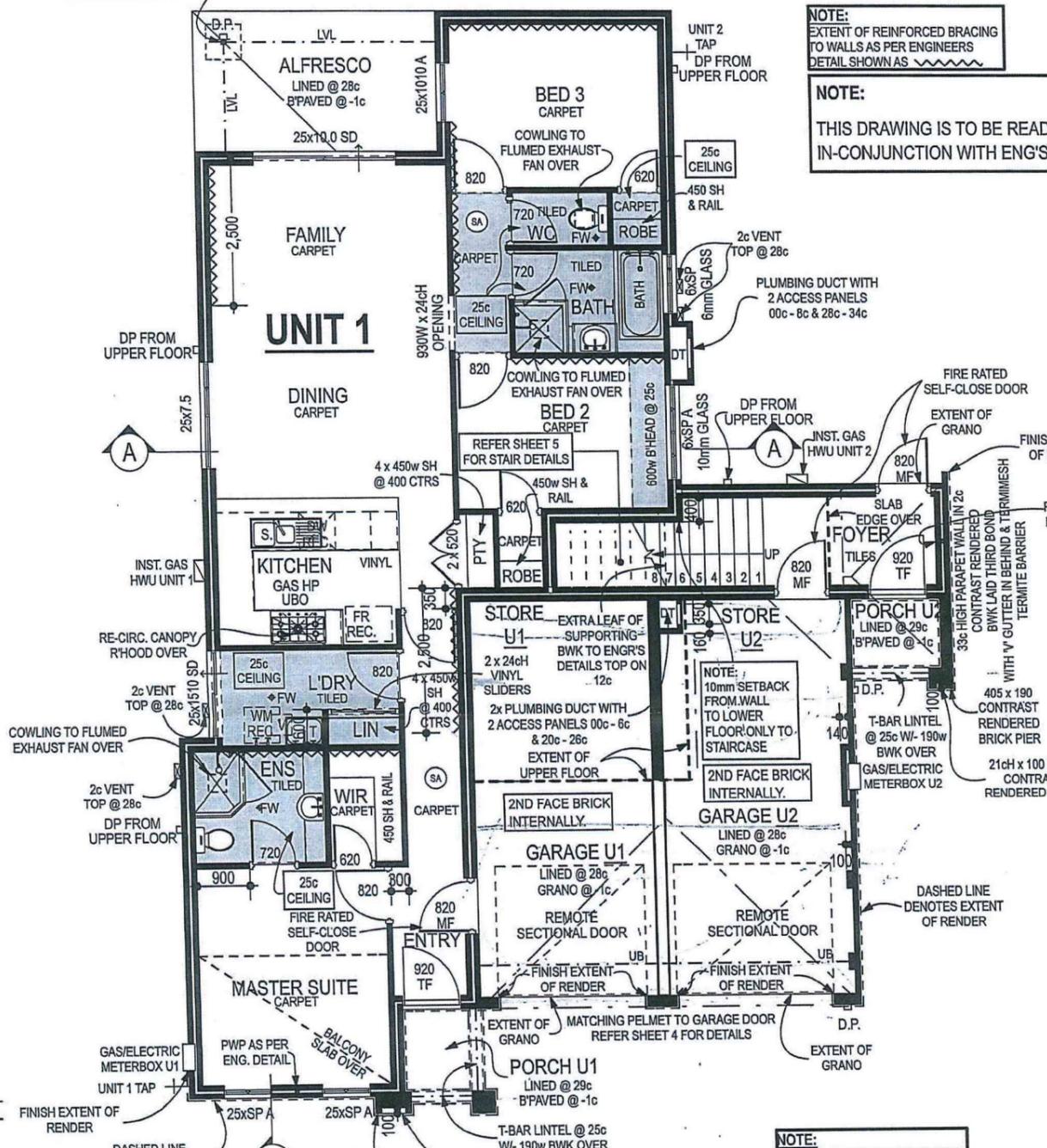
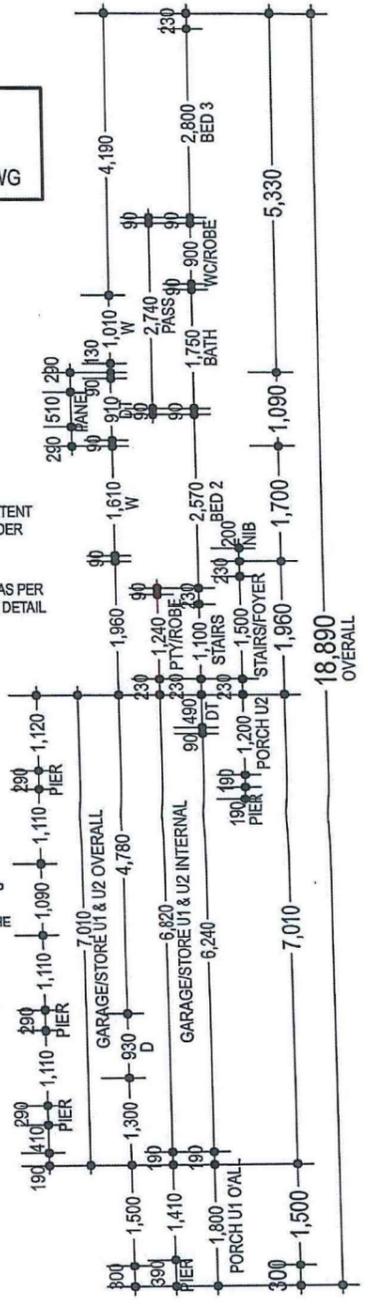
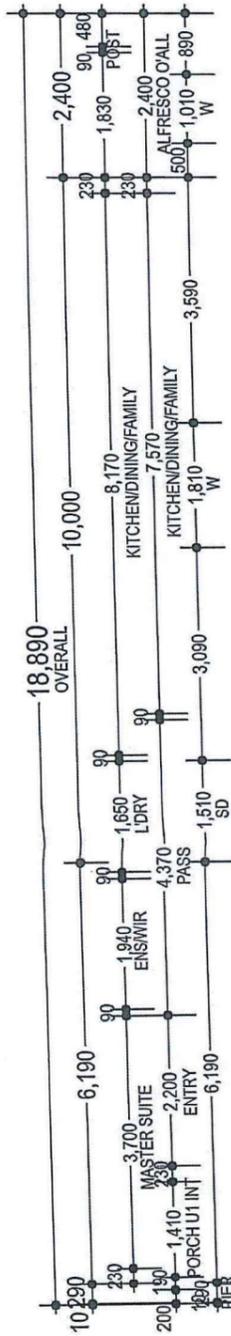
90 x 90 DURAGAL POST IN 600 x 600 x 280 DEEP MASS CONC. PAD FTG.

NOTE:

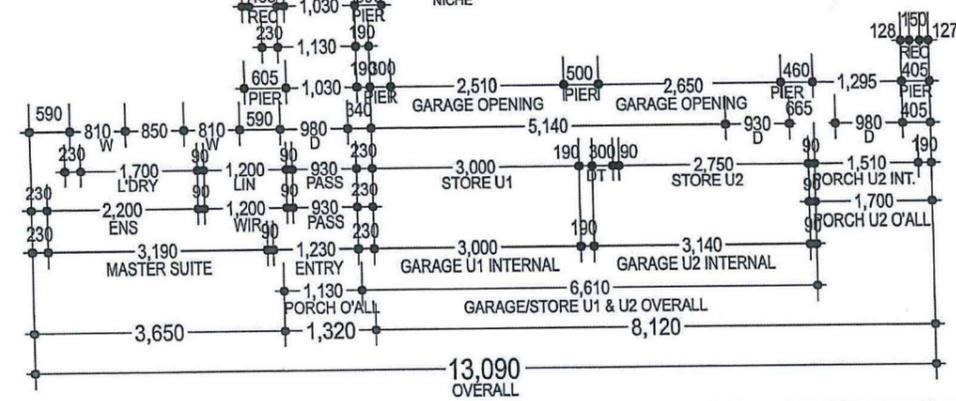
EXTENT OF REINFORCED BRACING TO WALLS AS PER ENGINEERS DETAIL SHOWN AS

NOTE:

THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG



GROUND FLOOR PLAN
SCALE 1:100



230 CAVITY BRICKWORK THIRD BOND FINISH EXTERNALLY

NOTE:

- TOP OF FOOTINGS FOR PIERS TO PORCH, GARAGE & STORE ARE TO BE 2c BELOW FINISHED PAVING LEVEL.
- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 29c THROUGHOUT GROUND FLOOR UNLESS NOTED OTHERWISE.



22 MOUNT STREET PERTH PH: 9261 1888
REG. BUILDER N°: 9769
A.B.N. 54 052 543 450
BGC RESIDENTIAL PTY LTD

REVISION	VO #	DRN	DATE	CHK
		CAR	30.01.13	RR
		RR	19.03.13	RR

CLIENT:
DATE: DEPARTMENT OF PLANNING
CLIENT:
DATE: 30 MAY 2013
BUILDER: FILE
DATE:

CLIENT: D.O.H
ADDRESS: LOT 1 (#12) (UNIT 1 & 2) COBHAM AVENUE NOLLAMARA

ONE-OFF	
MODEL N°	DATE
MAP REF.	WIND RATING
312 24 64	N1
COASTAL CATEGORY	ENGINEERS DETAIL
4	D10W
HOME ID	SHEET N°
80970	1 OF 18

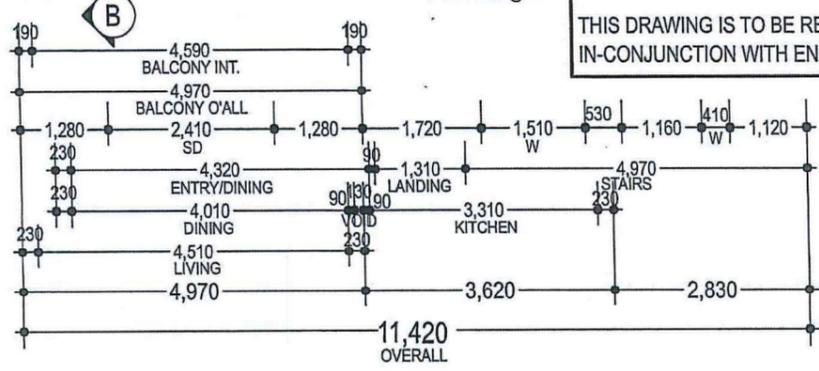
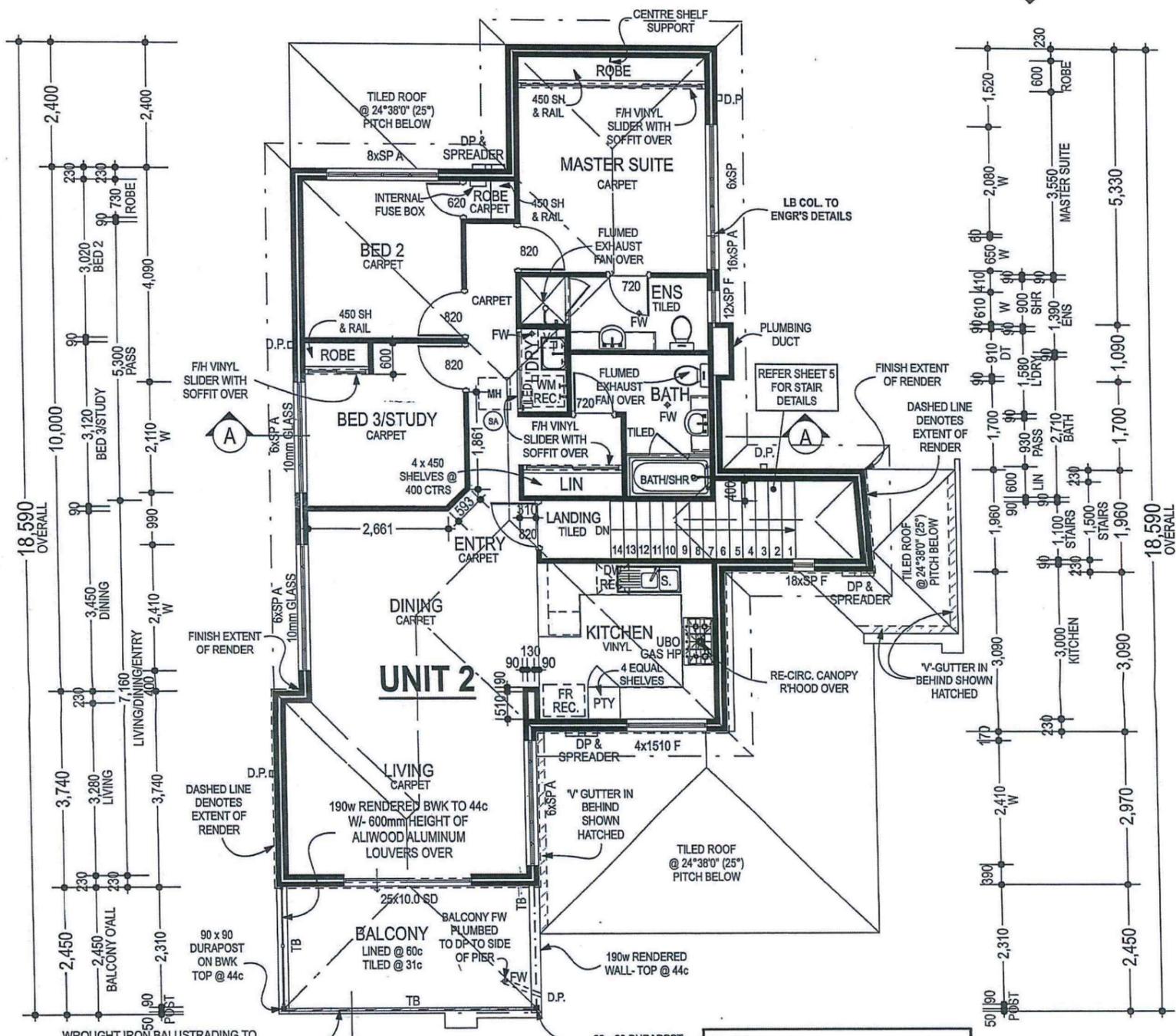
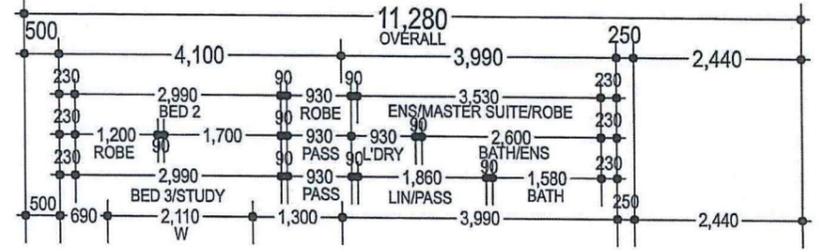
NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

SA DENOTES SMOKE ALARM

PLUMBING NOTE:
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

AREAS: UPPER FLOOR		
	PERIM. (m)	AREA (m2)
HOUSE AREA (Upper)	54.78	117.06
BALCONY	14.84	12.18
		129.24 m2
UPPER ROOF AREAS		
Material Type	Area [m2 on the flat]	
Roof - Tiled	150.41	



NOTE:
THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG

230 CAVITY BRICKWORK THIRD BOND FINISH EXTERNALLY

NOTE:

- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 28c + PL THROUGHOUT UPPER FLOOR UNLESS NOTED OTHERWISE.

UPPER FLOOR PLAN
SCALE 1:100



REVISION	VO #	DRN	DATE	CHK
ENG. DETAILS		RR	19.03.13	RR

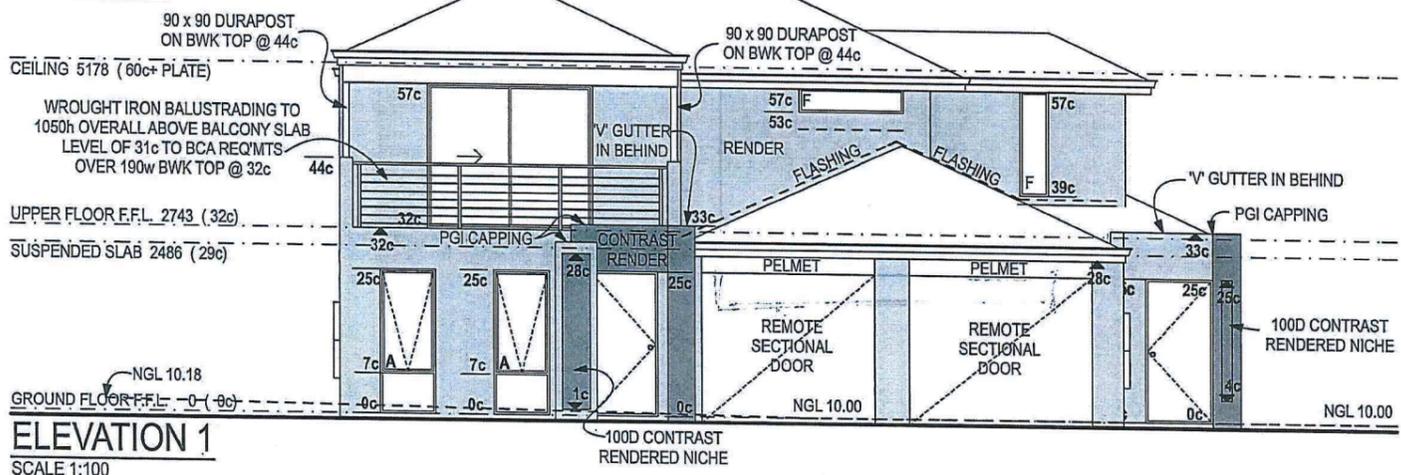
CLIENT:
DATE:
CLIENT:
DATE:
BUILDER:
DATE:

DEPARTMENT OF PLANNING
30 MAY 2013
20-50459-1
FILE

CLIENT: D.O.H
ADDRESS: LOT 1 (#12) (UNIT 1 & 2) COBHAM AVENUE NOLLAMARA

ONE-OFF	
MODEL N°	DATE 19.03.13
MAP REF. 312 24 64	WIND RATING N1
COASTAL CATEGORY 4	ENGINEERS DETAIL D10W
HOME ID 80970	SHEET N° 2 OF 18

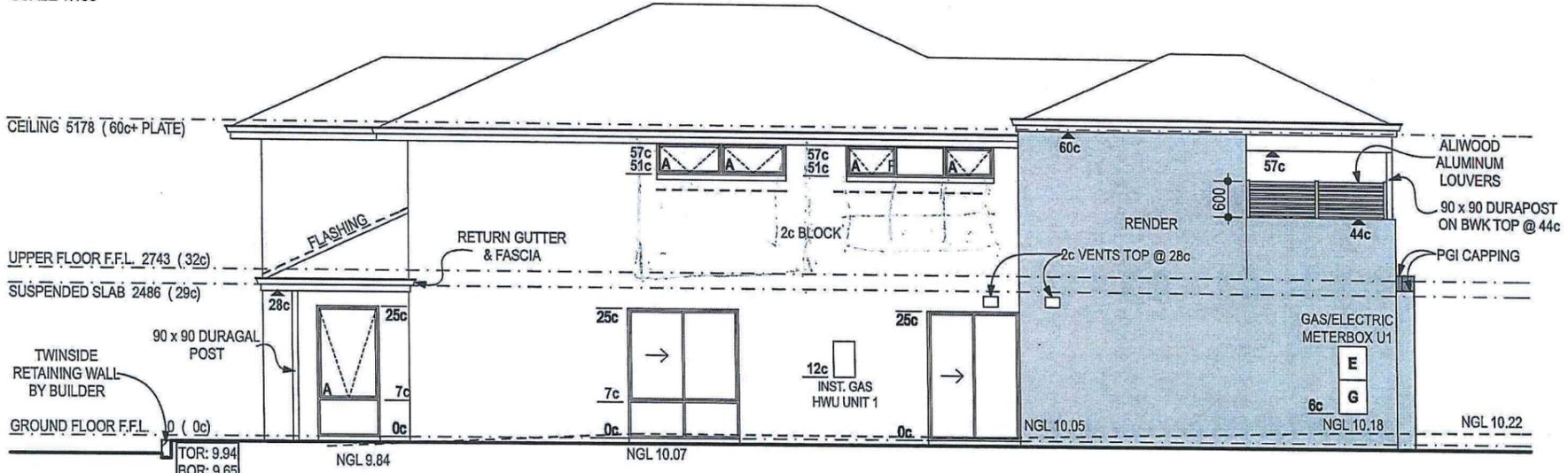
TILED ROOF ON 25°38'0" (25°) PITCH.



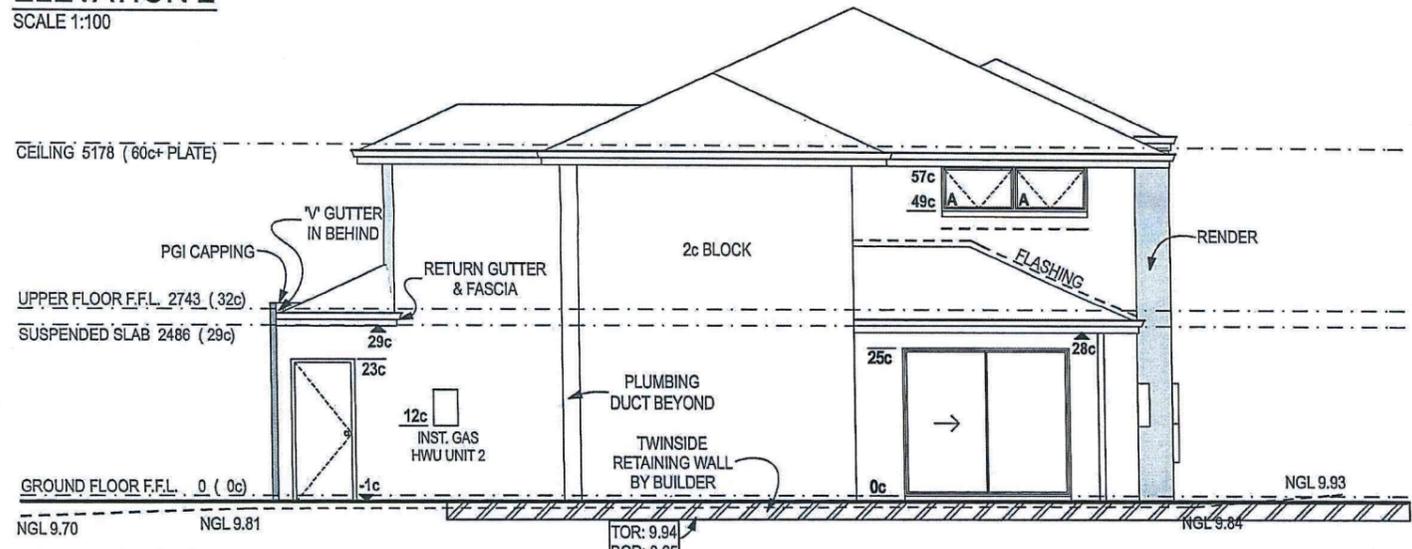
NOTE:

- EXTENT OF PAINTED SAND FINISH RENDER SHOWN SHADED, DARKER SHADE DENOTES CONTRAST RENDER.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- THE BUILDER RESERVES THE RIGHT TO FORM A 'V' JOINT TO EXTERNAL WALLS AROUND UPPER FLOOR LEVELS.
- PROVIDE FLASHING TO ALL UPPER FLOOR WINDOWS AS INDICATED DASHED LINE

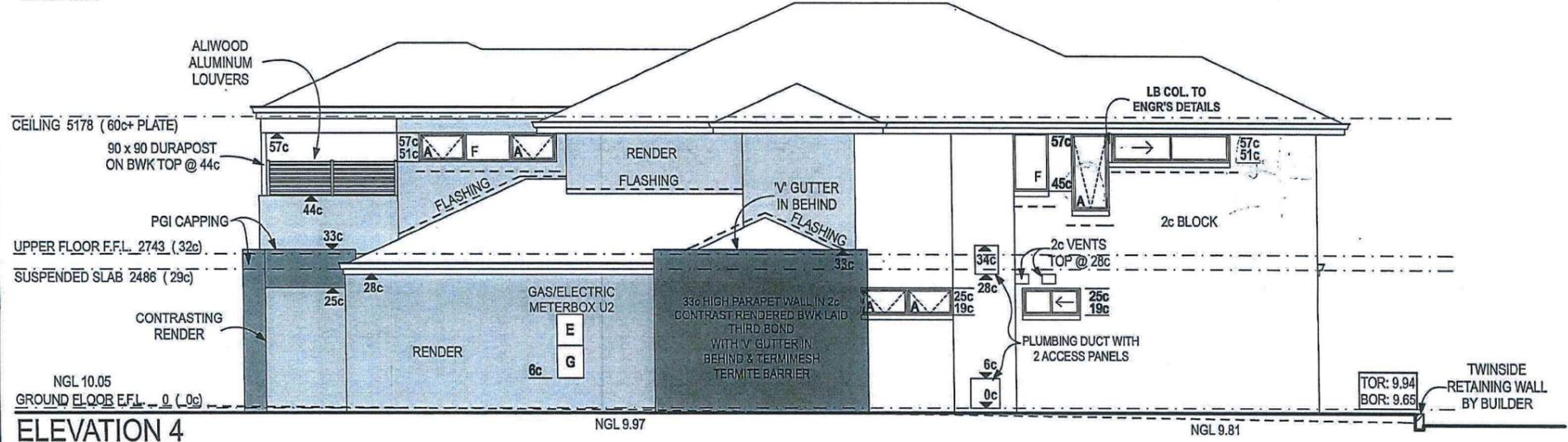
ELEVATION 1
SCALE 1:100



ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100



ELEVATION 4
SCALE 1:100

REVISION	VO #	DRN	DATE	CHK
		CAR	30.01.13	
		RR	19.03.13	RR

Sub-contractors to verify all dimensions on site.

CLIENT: DEPARTMENT OF PLANNING
 DATE: 3.0 MAY 2013
 CLIENT: 20-50459-1
 DATE: FILE
 BUILDER:
 DATE:

CLIENT: D.O.H
 ADDRESS: LOT 1 (#12) (UNIT 1 & 2) COBHAM AVENUE NOLLAMARA

© COPYRIGHT

ONE-OFF

MODEL N°	DATE
MAP REF.	WIND RATING
312 24 64	N1
COASTAL CATEGORY	ENGINEERS DETAIL
4	D10W
HOME ID	SHEET N°
80970	3 OF 18

NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

230 CAVITY BRICKWORK
THIRD BOND FINISH EXTERNALLY

AREAS: GROUND FLOOR		
	PERIM. (m)	AREA (m2)
HOUSE AREA (Ground)	64.42	111.42
ALFRESCO	18.40	19.47
GARAGE U4	18.02	18.91
GARAGE U3	17.54	17.55
STORE U3	9.24	4.73
STORE U4	8.66	4.28
PORCH U4	7.42	3.02
PORCH U3	5.16	1.32
		180.70 m2
GROUND ROOF AREAS		
Material Type	Area [m2 on the flat]	
Roof Tiled	52.30	

(SA) DENOTES SMOKE ALARM

NOTE:

27c CEILINGS TO ENTRY/FAMILY/DINING, KITCHEN & BED 2 TO HAVE 13mm PLASTERBOARD WITH RESILIENT FURRING CHANNEL WITH 120mm CAVITY AND 75mm GLASSWOOL INSULATION.

NOTE:

- TOP OF FOOTINGS FOR PIERS TO PORCH, GARAGE & STORE ARE TO BE 2c BELOW FINISHED PAVING LEVEL.
- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 29c THROUGHOUT GROUND FLOOR UNLESS NOTED OTHERWISE.

PLUMBING NOTE:

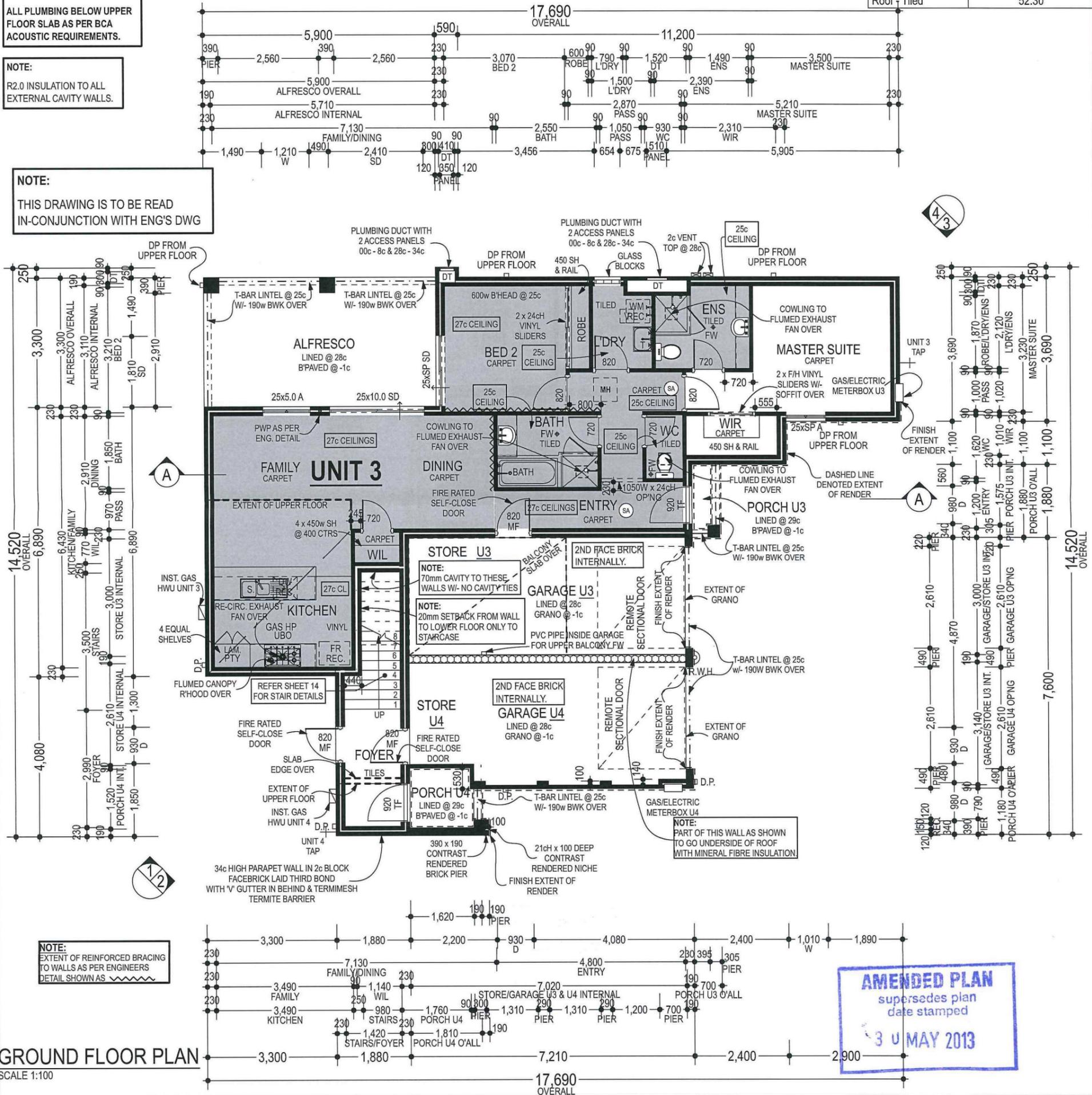
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

NOTE:

R2.0 INSULATION TO ALL EXTERNAL CAVITY WALLS.

NOTE:

THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG



GROUND FLOOR PLAN
SCALE 1:100

AMENDED PLAN
supersedes plan
date stamped
30 MAY 2013

NOW living

LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENG. DETAILS		RR	19.03.13	RR
AMENDED		MC	06.01.14	MC

CLIENT:
 DATE:
 BUILDER:
 DATE:

CLIENT: DOH
 ADDRESS: LOT 1 (#12)
 (UNIT 3 & 4)
 COBHAM AVENUE
 NOLLAMARA

ONE-OFF	
MODEL N°	DATE
90002	06.01.14
MAP REF.	WIND RATING
312 24 64	N1
COASTAL CATEGORY	ENGINEERS DETAIL
4	D10W
HOME ID	SHEET N°
80971	10 OF 18

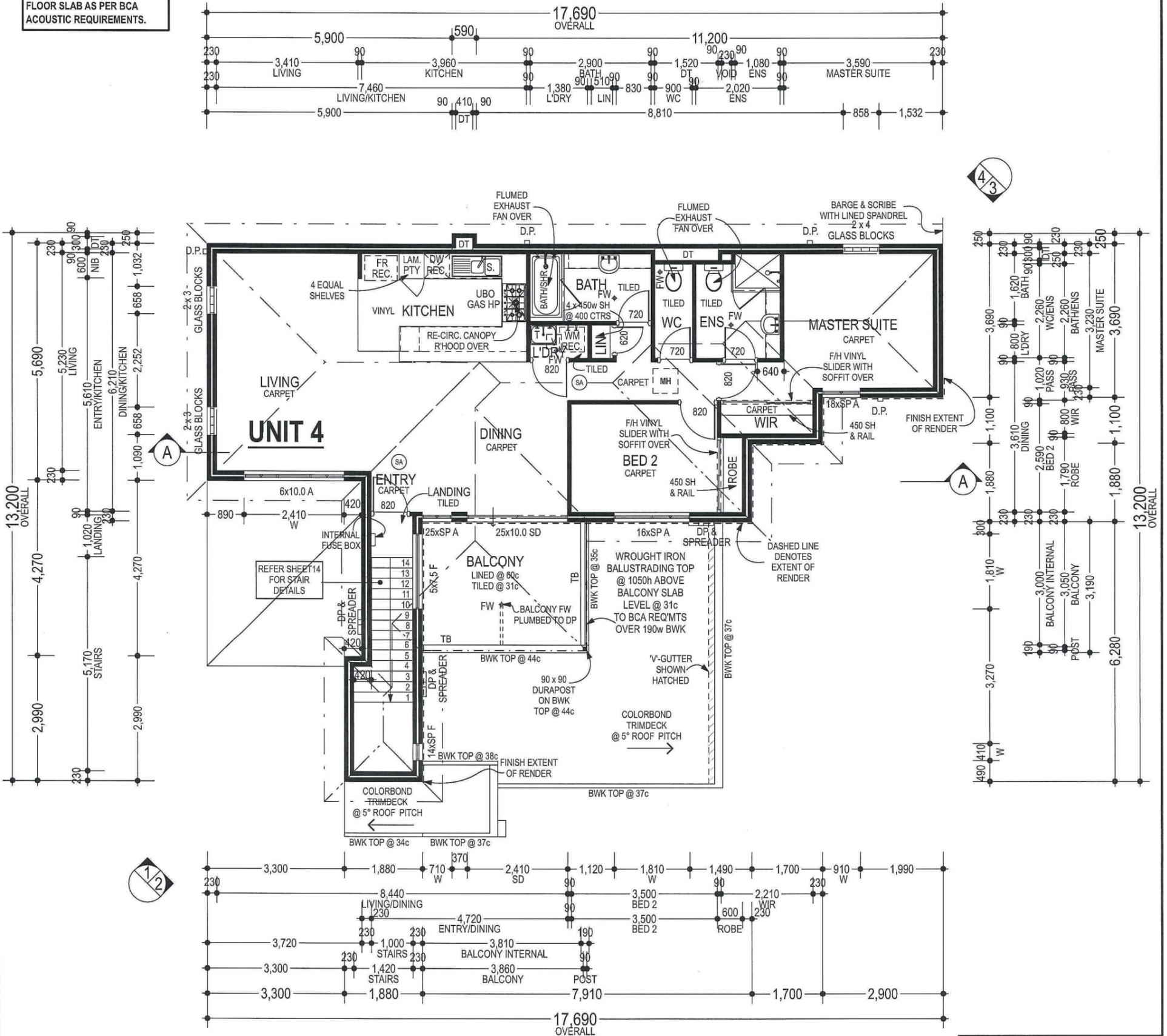
AREAS: UPPER FLOOR		
	PERIM. (m)	AREA (m ²)
HOUSE AREA (Upper)	62.62	113.08
BALCONY	14.38	12.76
		125.84 m ²
UPPER ROOF AREAS		
Material Type	Area [m ² on the flat]	
Roof - Tiled	149.85	

NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R6.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

(SA) DENOTES SMOKE ALARM

PLUMBING NOTE:
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.



NOTE:
THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG

AMENDED PLAN
supersedes plan
date stamped
3 0 MAY 2013

230 CAVITY BRICKWORK
THIRD BOND FINISH EXTERNALLY

NOTE:

- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 28c + PL THROUGHOUT UPPER FLOOR UNLESS NOTED OTHERWISE.

UPPER FLOOR PLAN
SCALE 1:100

NOW living
LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N^o: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENG. DETAILS		RR	19.03.13	RR
AMENDED		MC	06.01.14	MC

CLIENT:
DATE: DEPARTMENT OF PLANNING
CLIENT:
DATE: 06 JAN 2013
BUILDER: FILE 20-50459-1
DATE:

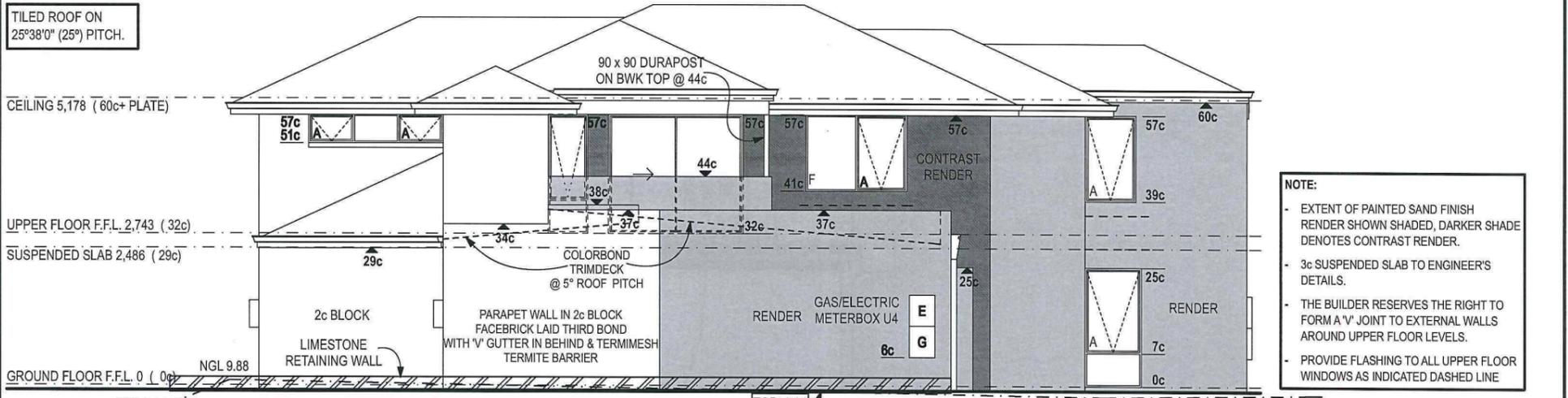
CLIENT: DOH
ADDRESS: LOT 1 (#12)
(UNIT 3 & 4)
COBHAM AVENUE
NOLLAMARA

© COPYRIGHT

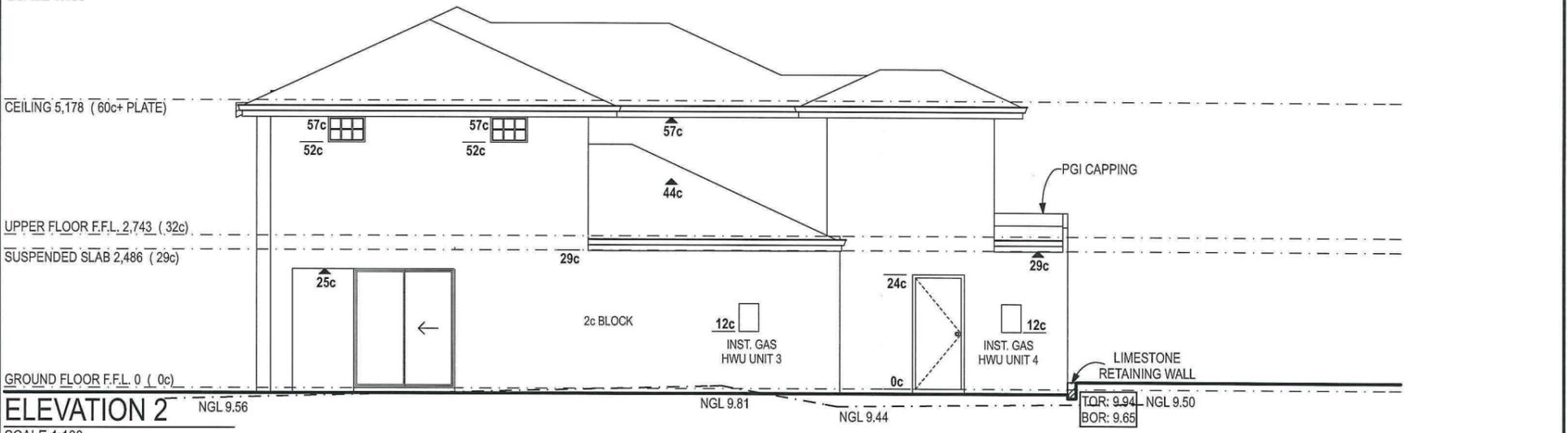
ONE-OFF

MODEL N ^o	DATE
90002	06.01.14
MAP REF.	WIND RATING
312 24 64	N1
COASTAL CATEGORY	ENGINEERS DETAIL
4	D10W
HOME ID	SHEET N ^o
80971	11 OF 18

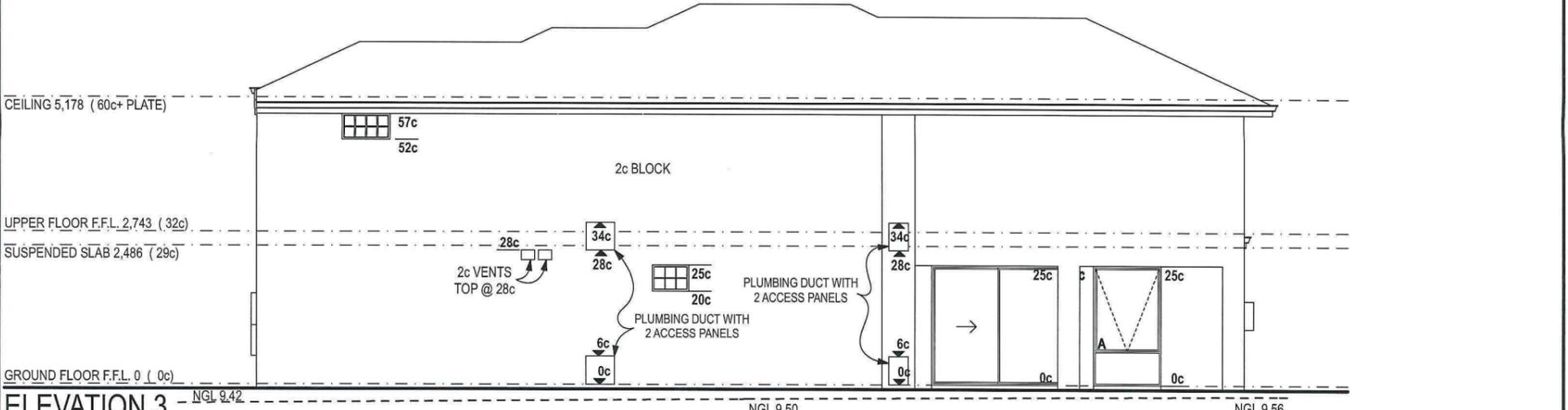
TILED ROOF ON
25°38'0" (25°) PITCH.



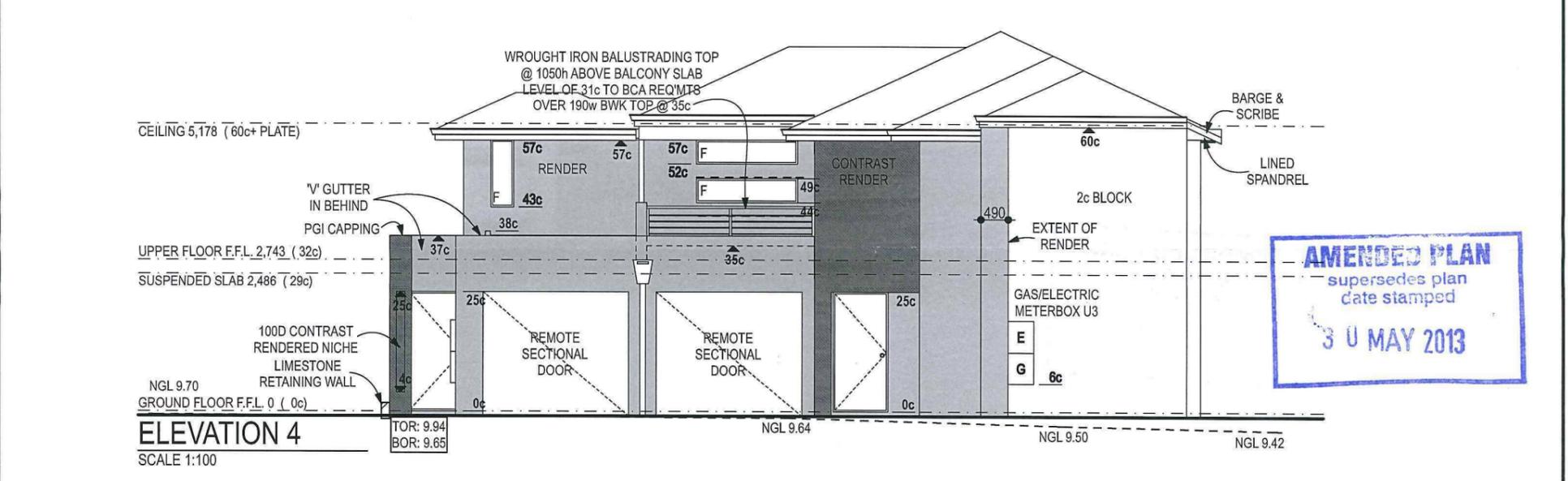
ELEVATION 1
SCALE 1:100



ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100



ELEVATION 4
SCALE 1:100

NOW living.
LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENG. DETAILS		RR	19.03.13	RR
AMENDED		MC	06.01.14	MC

CLIENT:
DATE: **DEPARTMENT OF PLANNING**
CLIENT:
DATE:
BUILDER: **FILE 20-50459-1**
DATE:

CLIENT: DOH
ADDRESS: LOT 1 (#12)
(UNIT 3 & 4)
COBHAM AVENUE
NOLLAMARA

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ONE-OFF

MODEL N°	DATE
90002	06.01.14
MAP REF.	WIND RATING
312 24 64	N1
COASTAL CATEGORY	ENGINEERS DETAIL
4	D10W
HOME ID	SHEET N°
80971	12 OF 18

NOTE:
THIS DRAWING IS TO BE READ
IN-CONJUNCTION WITH ENG'S DWG

TILED ROOF ON
25°38'0" (25°) PITCH.

CEILING 5178 (60c+ PLATE)

480 BOXED &
LINED EAVES

LIVING

ENTRY/DINING
(KITCHEN BEHIND)

BED 2
UNIT 4

ROBE

480 BOXED &
LINED EAVES

RENDER

3c SUSPENDED SLAB
REFER TO ENGINEER'S
DETAILS

UPPER FLOOR F.F.L. 2743 (.32c)

SUSPENDED SLAB 2486 (29c)

CEILING 2314 (27c)

FAMILY/DINING

UNIT 3

ENTRY

PORCH
U3

T-BAR LINTEL
@ 25c

B'PAVED
@ -1c

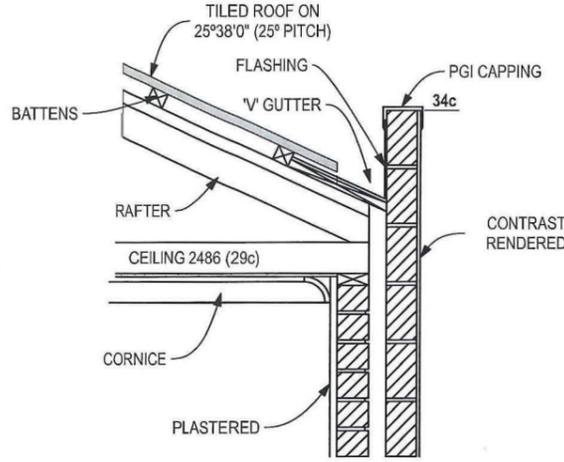
NOTE:
27c CEILINGS TO
ENTRY/FAMILY/DINING, KITCHEN & BED
2 TO HAVE 13mm PLASTERBOARD WITH
RESILIENT FURRING CHANNEL WITH
120mm CAVITY AND 75mm GLASSWOOL
INSULATION.

GROUND FLOOR F.F.L. 0 (0c)

SECTION A-A
SCALE 1: 50

REFER TO ENGINEER'S DETAILS FOR CONCRETE
FOOTINGS, SLAB & SAND PAD REQUIREMENTS

WATERPROOF MEMBRANE
ON COMPACTED SAND FILL



FOYER U4 PARAPET WALL DETAIL
SCALE 1: 20

AMENDED PLAN
supersedes plan
date stamped
30 MAY 2013

© COPYRIGHT

ONE-OFF	MODEL N°	90002	DATE	06.01.14
	MAP REF.	312 24 64	WIND RATING	N1
	CONSTANT CATEGORY	4	ENGINEERS DETAIL	D10W
	HOME ID	80971	SHEET N°	13 OF 18

CLIENT: DOH
ADDRESS: LOT 1 (#12)
(UNIT 3 & 4)
COBHAM AVENUE
NOLLAMARA

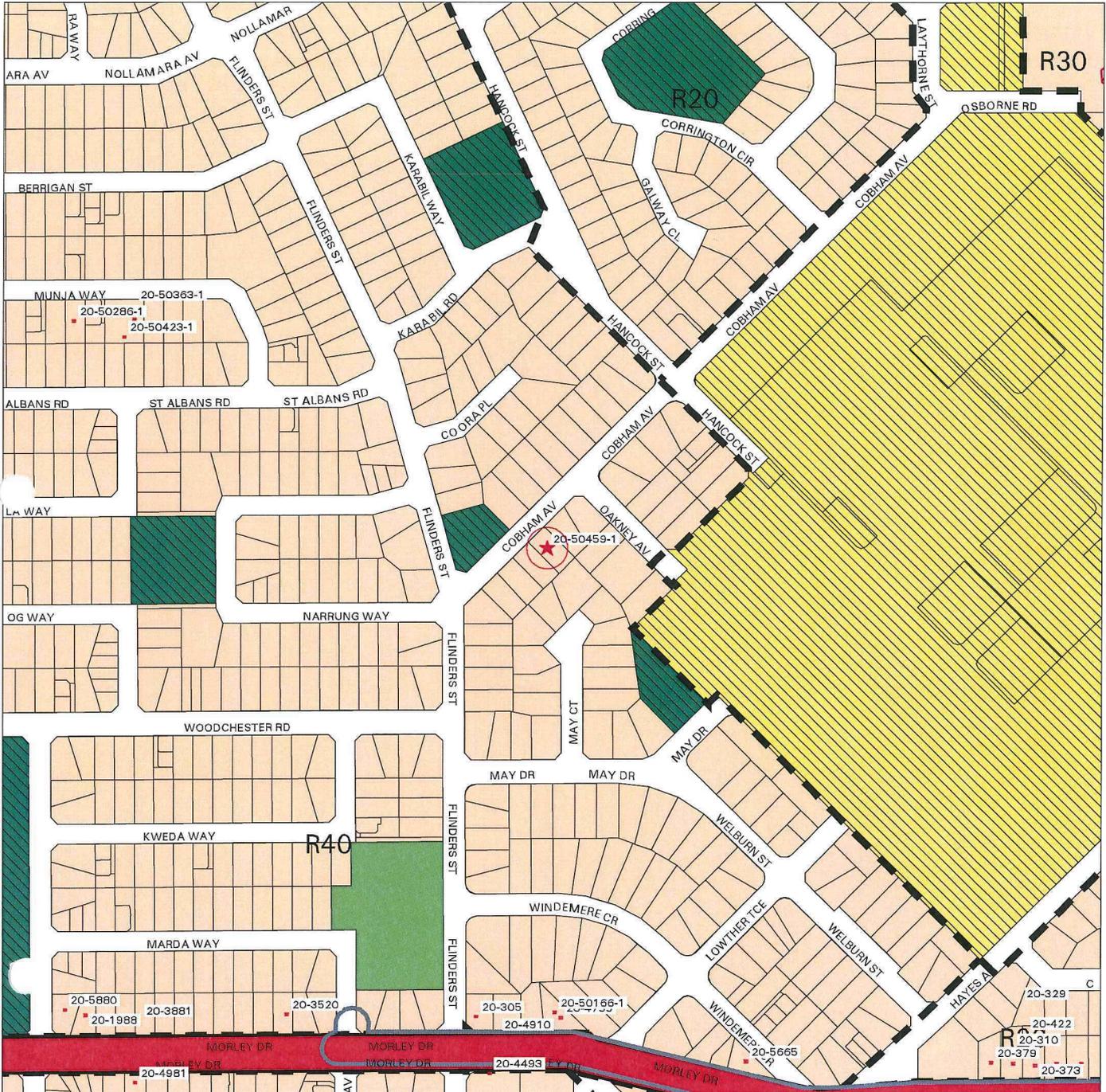
CLIENT: DATE: CLIENT: DATE: BUILDER: DATE:

DEPARTMENT OF PLANNING
60 JAN 2014
111 F. 20-50459-1

REVISION	VO #	DRN	DATE	CHK
ENG. DETAILS		CAR	30.01.13	RR
AMENDED		RR	19.03.13	RR
		MC	06.01.14	MC

now living®
LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 098 063076

Sub-contractors to verify all dimensions on site.
ORIGINAL



Development Application 20-50459-1 (MGA ref 391271mE 6471755mN Zone 50)

This data is to be used for the processing of subdivision applications only.

-  APPROX LOCATION OF APPLICATION
-  PRIMARY REGIONAL ROADS
-  CADASTRAL BOUNDARY
-  WESTNET ENERGY GAS PIPELINE
-  BUSH FOREVER 2000 SITES
-  DEVELOPMENT APPLICATION
-  R CODE BOUNDARY
-  RESIDENTIAL
-  PUBLIC OPEN SPACE
-  PRIVATE INSTITUTION
-  PUBLIC USE UTILITY

141924d.eps

ITEM NO: 9.4

One Grouped Dwelling and Four Multiple Dwellings - Lot 4 (No. 10) Vermont Street, Nollamara

WAPC OR COMMITTEE: Statutory Planning Committee

REPORTING AGENCY: Department of Planning
REPORTING OFFICER: Senior Planner, Metropolitan Central
AUTHORISING OFFICER: Director, Metropolitan Central
AGENDA PART: G
FILE NO: 20-50458-1
DATE: 11 March 2014
ATTACHMENT(S): 1. Development Plans
2. Location and Zoning Plan
3. Aerial Photo
REGION SCHEME ZONING: MRS: Urban
LOCAL GOVERNMENT: City of Stirling
LOCAL SCHEME ZONING: Residential R40
LGA RECOMMENDATION(S): Refusal
REGION DESCRIPTOR: Perth Metro Central
RECEIPT DATE: 30 May 2013
PROCESS DAYS: 285
APPLICATION TYPE: Development
CADASTRAL REFERENCE: Lot 4 (No. 10) Vermont Street, Nollamara

RECOMMENDATION:

That the Western Australian Planning Commission resolves to approve the proposed development of one grouped dwelling and four multiple dwellings at Lot 4 (No. 10) Vermont Street, Nollamara, subject to the following conditions:

- 1. The development shall be carried out only in accordance with the terms of the application as approved herein, and the attached plans (Site Plan, Unit 2&3 Layout Plans and Elevations) date-stamped 21 January 2014, and attached plans (Unit 4&5 Layout Plans and Elevations) date stamped 17 December 2013, and the attached plans (Unit 1 Layout Plans and Elevations) date stamped 11 October 2013 by the Department of Planning on behalf of the Western Australian Planning Commission.***
- 2. Prior to the issue of a building permit, the plans are to be amended to show:***

-
- (i) *a maximum internal driveway taper of 1:5 on the south-western side of Unit's 1's driveway; and*
- (ii) *the proposed crossover wings for the two proposed crossovers are to comply with the City of Stirling's Engineering Department specification requiring 1.8m x 1.8m crossover wings;*
to the specification of the local government and the satisfaction of the Western Australian Planning Commission.
3. *Gradients to driveways and ramps serving parking areas are to comply with Australian Standard 2890.1, and no changes to verge levels are permitted, to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
4. *All driveways, parking, manoeuvring and circulation areas are to be constructed of brick paving (or alternative finishes such as concrete or bitumen are acceptable if it has a decorative type finish) and sealed, drained and maintained to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
5. *Vehicular parking, manoeuvring and circulation areas indicated on the approved plan being sealed and drained, the five (5) parking spaces being marked out and maintained in good repair to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
6. *Any existing crossovers not included as part of the proposed development on the approved plan are to be removed and new kerbing and verge to be reinstated, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
7. *Any on-site clothes drying facilities being screened from public view, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
8. *The one visitor parking space being provided on site is to be permanently marked for the exclusive use of visitor's parking to the specification of the local government and the satisfaction Western Australian Planning Commission.*
9. *Pedestrian pathways providing wheelchair accessibility connecting all entries to buildings with a public footpath and car parking areas to the specification of the local government and the satisfaction of the Western Australian Planning Commission.*
10. *Adequate lighting being provided to communal pathways and parking areas, to the specification of the local government and the satisfaction Western Australian Planning Commission.*
11. *The boundary walls/s not to exceed the height depicted on the approved plans, and the surface finish of the wall facing a neighbour should be to*
-

the satisfaction of the adjoining neighbour, to the specification of the local government and the satisfaction Western Australian Planning Commission.

12. No walls, fences or letterboxes above 0.75 metres in height to be constructed within 1.5 metres of where:

- (i) walls or fences adjoin vehicular access points to the site; or**
- (ii) a driveway meets a public street; or**
- (iii) two streets intersect;**

unless the further approval of Council is obtained, to the specification of the local government and the satisfaction Western Australian Planning Commission.

13. All land indicated as landscaped area on the approved plan being developed on practical completion of the building/s, and all landscaped areas maintained in good condition thereafter, to the specification of the local government and the satisfaction Western Australian Planning Commission.

14. All eaves to the proposed development to maintain a minimum setback of 750mm from the boundary, to the specification of the local government and the satisfaction Western Australian Planning Commission.

15. Stormwater from all roofed and paved areas to be collected and contained on site, to the specification of the local government and the satisfaction Western Australian Planning Commission.

ADVICE TO APPLICANT

1. Where an approval has lapsed, no development shall be carried out without further approval having been first sought and obtained.

2. The vehicular access shown in this application has been assessed and determined based upon the location of street trees as shown on the submitted plans. It is the responsibility of the applicant to ensure that this information is correct as any inaccuracy of the plans will not be considered justification for removal of the trees in the event that their positions are incorrectly shown. Removal of street trees without the written approval of the City of Stirling is an offence.

3. This is not a building licence or an approval to commence or carry out development under any other law. It is the responsibility of the applicant to obtain any other necessary approvals, consents and licences required under any other law, and to commence and carry out development in accordance with all relevant laws.

-
4. ***This approval is not an authority to ignore any constraint to development on the land, which may exist through statute, regulation, contract or on title, such as an easement or restrictive covenant. It is the responsibility of the applicant to investigate any such constraints before commencing development. This approval will not necessarily have regard to any such constraint to development.***
 5. ***Noisy Construction Work outside the period 7:00am to 7:00pm Monday to Saturday and at any time on Sundays and Public Holidays is not permitted unless an approved Noise Management Plan for the construction site has been issued.***
 6. ***The street address for the development will be: house number 12 Vermont Street, Nollamara and house numbers 1/10, 2/10, 3/10., and 4/10 Vermont Street.***
 7. ***All street trees located in verge areas adjoining the subject lots are to be retained and protected.***
 8. ***All boundary fencing behind the front setback line is to accord with the provisions of the City's Local Laws pertaining to the provision of a sufficient fence.***

SUMMARY:

The proposal is for one grouped dwelling and four multiple dwellings, to be constructed by the Department of Housing (DOH). The application is required to be determined by the Western Australian Planning Commission (WAPC) as the recommendation is inconsistent with the recommendation of the local government and the proposal is not consistent with the deemed to comply provision of the Residential Design Codes (R-Codes) with regard to setbacks and storerooms.

LEGISLATION / STRATEGIC PLAN / POLICY:

Legislation Development Approval - Part 10 of the P&D Act 2005
Section: Section 162

Strategic Plan
Strategic Goal: Planning
Outcomes: Effective Delivery of Integrated Plans
Strategies: Implement State and Regional Planning Priorities

Policy
Number and / or Name: State Planning Policy 3.1 Residential Design Codes

INTRODUCTION:

The subject application submitted by the DOH seeks approval to construct one grouped dwelling and four multiple dwellings on a 1,042m² lot (**Attachment 1 - Development Plans**). The proposed development is estimated to cost \$1,150,000 and the estimated time of completion is approximately November 2014.

The site is zoned 'Residential' with an R40 density coding under the City of Stirling Local Planning Scheme No. 3 (LPS3) and 'Urban' under the Metropolitan Region Scheme (MRS) (**Attachment 2 - Location and Zoning Plan**).

Pursuant to the provisions of the *Planning and Development Act 2005*, public works by public authorities do not require the approval of the local government under the planning scheme but require the approval of the WAPC under the MRS.

The WAPC has granted delegated authority for the DoH to determine its own applications under the MRS where such applications comply with the City's Town Planning Scheme/Codes/Policy and the proposed construction of no more than 10 dwellings of a height of two storey or less. The City of Stirling has advised that the proposed development does not comply and has referred the application to the WAPC for determination.

In accordance with the provisions of the MRS, in determining the merits of a development application, the WAPC is required to have regard to orderly and proper planning, the preservation of the amenities of the locality and the purpose of the land's zoning.

CONSULTATION:

The City recommends refusal of the development application for the following reasons:

- (i) Clause 6.1.1 'Building Size' - Plot ratio is proposed to be 0.683 or 555m² in lieu of 0.6 or 487m².
- (ii) Clause 6.1.4 'Side and Rear Setbacks' - Unit 4 - The proposed master suite, kitchen, dining and family room is proposed to be setback 1.3m from the northern boundary in lieu of 1.5m.
The proposed alfresco area for Unit 4 is setback 0.9m in lieu of 1.5m.
Unit 5 - The proposed balcony, living, dining, bed 3 and 2 (bulk) is proposed to be setback 1.3m from the northern boundary in lieu of 1.8m
- (iii) Clause 6.3.2 'Landscaping' - The proposed landscaping of the front setback area is to be 31.14% in lieu of 50%.
- (iv) Clause 6.3.3 'On-site parking provision' - Location A - 4 proposed car parking bays for residents in lieu of 4.5 (5) car parking bays. No proposed bicycle bays for visitors in lieu of 1 bicycle bay. The proposed car parking is insufficient given the type and size of the dwellings, it is reasonable to conclude they are designed for families. As a result the proposed variations are unable to be supported by the City as it is considered inadequate for the expected parking needs of the future occupants and their visitors.

-
- (v) Clause 6.4.6 'Utilities and Facilities' - Unit 2, 3 and 4 internal storage dimension is proposed to be 1.4m in width in lieu of 1.5m in width.
 - (vi) Local Planning Policy 6.7 'Parking and Access' (LPP 6.7) - The turnaround area for the single garage for Unit 4 does not comply with Figure 3 under Clause 7.2.1.1 'Turning Circles (manoeuvring)' of the City's LPP 6.7 Policy.

The City has however provided recommended conditions should the WAPC be minded to approve the application.

COMMENTS:

While approval is limited to the MRS, the WAPC must still consult with the local government and give due regard to its local planning scheme and the principles of proper and orderly planning. However, the WAPC is primarily assessing this proposal under the MRS and not against LPS3. Notwithstanding this, and in acknowledgement of the City's objections, the following comments are made.

Building Size

The City of Stirling has included the car parking and garage areas in their calculation of plot ratio area. However, the definition of plot ratio area provided by the R-Codes specifically states that areas used exclusively for the parking of wheeled vehicles, storerooms, lobbies and balconies are excluded.

Calculating the plot ratio area using the definition provided by the R-Codes results in a total plot ratio area of 454.4m² which is less than the 0.6 ratio allowed (which would be 495m²). Therefore the proposed building size is acceptable.

Side and Rear Setbacks

The Unit 4 master suite, kitchen, dining and family room is set back 1.3m in lieu of 1.5m. The family room of Unit 4 is situated so that approximately 700mm of the length of the room is non-compliant, due to the shape of the lot. The applicant has advised they can step this part of the room however given the variation will have little impact on the bulk and scale of the development as viewed from the adjoining property and the street, and will result in a more functional habitable area, the variation is considered acceptable.

The proposed alfresco area for Unit 4 is setback 0.9m in lieu of 1.5m. The applicant has advised that although the paving is setback 0.9m, the pier of the alfresco is setback 1.5m. Therefore as defined by the R-Codes this setback complies.

The proposed balcony, living, dining, bed 3 and 2 of Unit 5 is proposed to be setback 1.3m from the northern boundary in lieu of 1.8m. A minor part of bed 2 is non-compliant, due to the shape of the lot. The applicant has advised they can step this part of the room however given the variation will have little impact on the bulk and scale of the development as viewed from the adjoining property and the street, and will result in a more functional habitable area, the variation is considered acceptable.

Landscaping

Amended plans have been provided showing 50% landscaping within the front setback area and therefore this element is now compliant.

Parking

The development provides five resident car parking bays (one per unit) and one visitor bay, which is consistent with the requirements of the R-Codes given that the proposed dwellings are located within 250m of a high frequency bus route.

The development site is within 250m of Wanneroo Road which has a high frequency bus route and is also within walking distance of Nollamara Avenue and Ravenswood Drive, all of which present further public transport facilities.

A bicycle bay has been provided on amended plans and therefore this element is now compliant.

Utilities and Facilities

The storerooms for Units 3, 4 and 5 provide 1.4m minimum dimension in lieu of the required 1.5m. This is considered a minor variation is acceptable given the storage is in the garage and therefore the garage edges can also be utilised for storage purposes.

Turnaround Area

Amended plans have been provided which have altered the vehicle access for the proposed development so that this element now complies.

Department of Housing Justification

The DOH has provided the following comments with regard to the proposed development:

- The Department's 'maisonette' units reflect contemporary housing options and are a move away from the conventional 'cookie cutter' style of infill development in the suburbs. It represents an innovative product that provides a broader choice of dwelling type and price points. The maisonettes are designed to maximise yield while blending with the surrounding properties as they 'present' as two-storey houses;
- It is anticipated that the subject proposal will continue to introduce two-storey residential dwellings into the Nollamara streetscape. This has been shown to encourage similar development over time, and will encourage a stronger mix of socio-economic groups in the community.
- The proposed design aims to follow the objectives of the City's Balga-Westminster-Nollamara Local Area Plan, particularly with regards to introducing a greater range of housing types aimed at improving housing affordability.

CONCLUSION:

The proposed development is for one two storey grouped dwelling and four multiple dwellings, comprising two-storey maisonette units with one dwelling per floor. The proposed buildings are not dissimilar to surrounding residential development. The proposal provides housing variety.

In conclusion it is considered that the proposed residential scheme is an appropriate development for the 'Urban' zone under the MRS. The proposal satisfactorily meets the City of Stirling Local Planning Scheme No. 3, the requirements of the Residential Design Codes 2013 and represents proper and orderly planning of the locality. Further, approval of the proposed multiple dwellings will not adversely affect the amenity of the locality and will provide an alternative form of housing.

It is therefore recommended that this application be approved subject to conditions.

WARNING!
BOUNDARY RE-PEG SURVEY
REQUIRED TO CONFIRM PEG POSITIONS



LOT 4

1042sqm
SANDY

NOTE: BOUNDARY POSITION AS PER ALIGNMENT REQUIRES REPEG SURVEY

NOTE: APPROXIMATELY 8 Km FROM COAST LINE

NOTE: ADD 35.50 TO ALL LEVELS ON PLAN TO OBTAIN APPROXIMATE A.H.D. LEVELS. THIS VALUE DERIVED FROM WATER CORP. AS CONSTRUCTED RECORDS & MAY NOT BE EXACT. AN A.H.D. SURVEY IS REQUIRED TO OBTAIN AN ACCURATE VALUE.

UNIT 1		
LOCATION	GRAND m ²	B'PAVED m ²
Porch	0.00	1.56
Garage, Drive, Store & Path	20.58	14.29
Crossover	0.00	19.96
UNIT 2 & UNIT 3		
LOCATION	GRAND m ²	B'PAVED m ²
Porch U2, Porch U3 & Alfresco	0.00	14.29
Garage U2, Garage U3, Store U2, Store U3, Path & Visitors Carbay	41.23	63.68
UNIT 4 & UNIT 5		
LOCATION	GRAND m ²	B'PAVED m ²
Porch U4, Porch U5 & Alfresco	0.00	13.03
Garage U4, Garage U5, Store U4, Store U5, Path & Reversing Bay	41.14	60.88
COMMON AREA		
LOCATION	GRAND m ²	B'PAVED m ²
Common Driveway	0.00	43.29
Common Crossover	0.00	45.55

NOTE: N1 WIND CLASSIFICATION. REFER TO ENGINEERS SPECIFICATION

NOTE: THE OWNER IS LIABLE FOR REMOVAL OF ANY BOUNDARY FENCES IF REQUIRED FOR CONSTRUCTION PURPOSES.

NOTE: STORMWATER DISPOSAL BY BUILDER TO COUNCIL REQUIREMENTS. REFER SHEET 27.

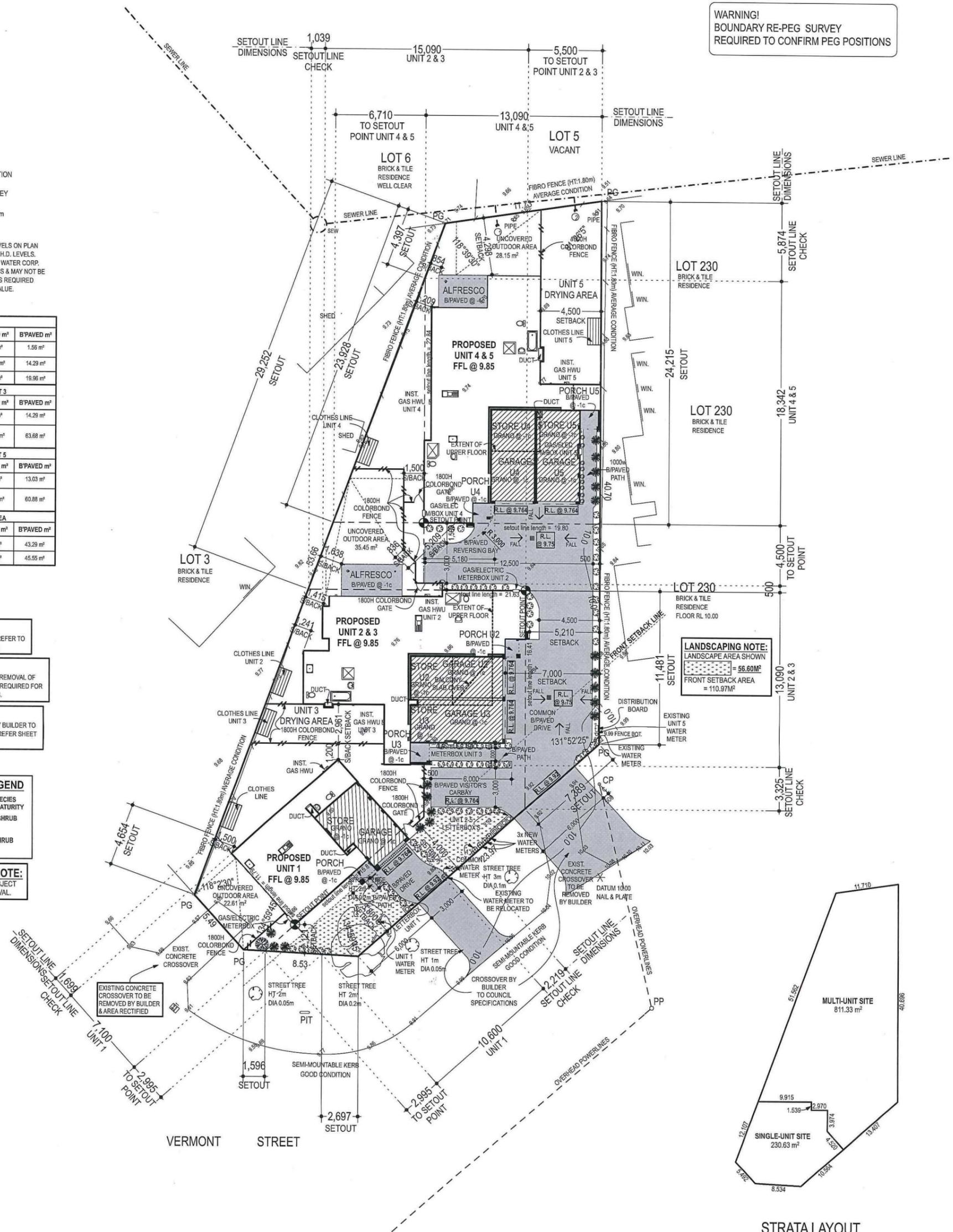
COASTAL CATEGORY 4

LANDSCAPE LEGEND

- GROUND COVER SPECIES 0-300mm HIGH AT MATURITY
- NATIVE PLANT OR SHRUB 500-1000mm HIGH
- NATIVE TREE OR SHRUB 2000-4000mm HIGH

LANDSCAPING NOTE:

LANDSCAPE LAYOUT SUBJECT TO DEVELOPERS APPROVAL.



LANDSCAPING NOTE:
LANDSCAPE AREA SHOWN
= 56.80m²
FRONT SETBACK AREA
= 110.97m²

SITE PLAN
SCALE 1:200

STRATA LAYOUT
NOT TO SCALE

SEW CONN POSITION APPROXIMATE ONLY
SEW INV. 7.95
UP. 0.9
DEPTH. 0.83m

SEW CONN POSITION APPROXIMATE ONLY
SEW INV. 7.91
UP. 0.9
DEPTH. 0.77m

NOTE: SEWER MH No. T9710 APPROXIMATELY 108.21m FROM DATUM (LOCAL LEVEL 9.24) (A.H.D. LEVEL 44.74)

DEPARTMENT OF PLANNING

27 JAN 2013
FILE 20-504 58-1

REVISION	VO #	DRN	DATE	CHK
SETING		JE	01.02.13	
AMENDED		MC	20.02.13	
AMENDED		MC	19.08.13	
AMENDED		MC	16.12.13	
AMENDED		MC	20.01.14	

Sub-contractors to verify all dimensions on site.

CLIENT: _____
DATE: _____
CLIENT: _____
DATE: _____
BUILDER: _____
DATE: _____

CLIENT	DEPARTMENT OF HOUSING	BUILDER	IMPRESSIONS
LOT	4 VERMONT STREET	AUTHORITY	STIRLING
SUBURB	NOLLAMARA	MAP REF.	312 23 65
PLAN	24074	C/T Vol.Fol.	2174 / 30
DATE OF SURVEY	05.02.13	SCALE	1:200 @ A2
JOB No.	OURS: 183631	YOURS:	81064 - 5

SERVICE INFORMATION		SEWERAGE	YES	PRELID	YES	LEFT
GAS	BTC	WATER	YES	POWER	O/H	BTC
AREA	ESTABLISHED	TELSTRA	YES	POWER	O/H	BTC
WATER	SV ₁ STOP VALVE	HY ₁ HYDRANT	FP ₁ FLUSH POINT	METER ₁	WATER METER	
SEWERAGE	SEW SEWER MANHOLE	IS ₁ INSPECTION SHAFT	IQ ₁ INSPECTION OPENING	GAS ₁ GAS ₁	GAS METER	
POWER	CP ₁ CONSUMER POLE	PS ₁ POWER POLE	LP ₁ LAMP POST	SP ₁ STAY POLE	PD ₁ POWER DOME	
STORMWATER	SW MANHOLE	GRATE	SIDE ENTRY PIT	TELSTRA	PIT	TELSTRA PIT
SURVEY	PF ₁ PEG FOUND	Pdist ₁ PEG DISTURBED	PG ₁ PEG GONE	STATION	*	DATUM / CONTROL

AUTOMATED SURVEYS
LICENSED SURVEYORS AND DEVELOPMENT CONSULTANTS

PTY LTD
3 Old Street West Perth WA 6005
P.O. Box 1548 West Perth WA 6872
Telephone: +61 (0)8 9214 1777
Facsimile: +61 (0)8 9214 1778

The information on this drawing is correct as at the date of survey. Surveyors are to be confirmed with relevant Authorities. This includes without limitation: Sewerage, Water Supply, Drainage, Power Supply, Gas Supply & Communications. Further interests / encumbrances may be listed on the Certificate of Title. Location of boundary is relative to fence or boundary markers is not guaranteed. Boundary is shown arbitrarily as per best fit. Roping Survey will be required. Copyright 2008 Automated Surveys Pty Ltd. All rights reserved.



NOTE:

- CLAY BRICKS TO ALL INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE, STORE & GARAGE.
- R1.5 FOIL INSULATION TO ALL EXTERNAL CAVITY WALLS OF BOTH GROUND AND UPPER FLOOR.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

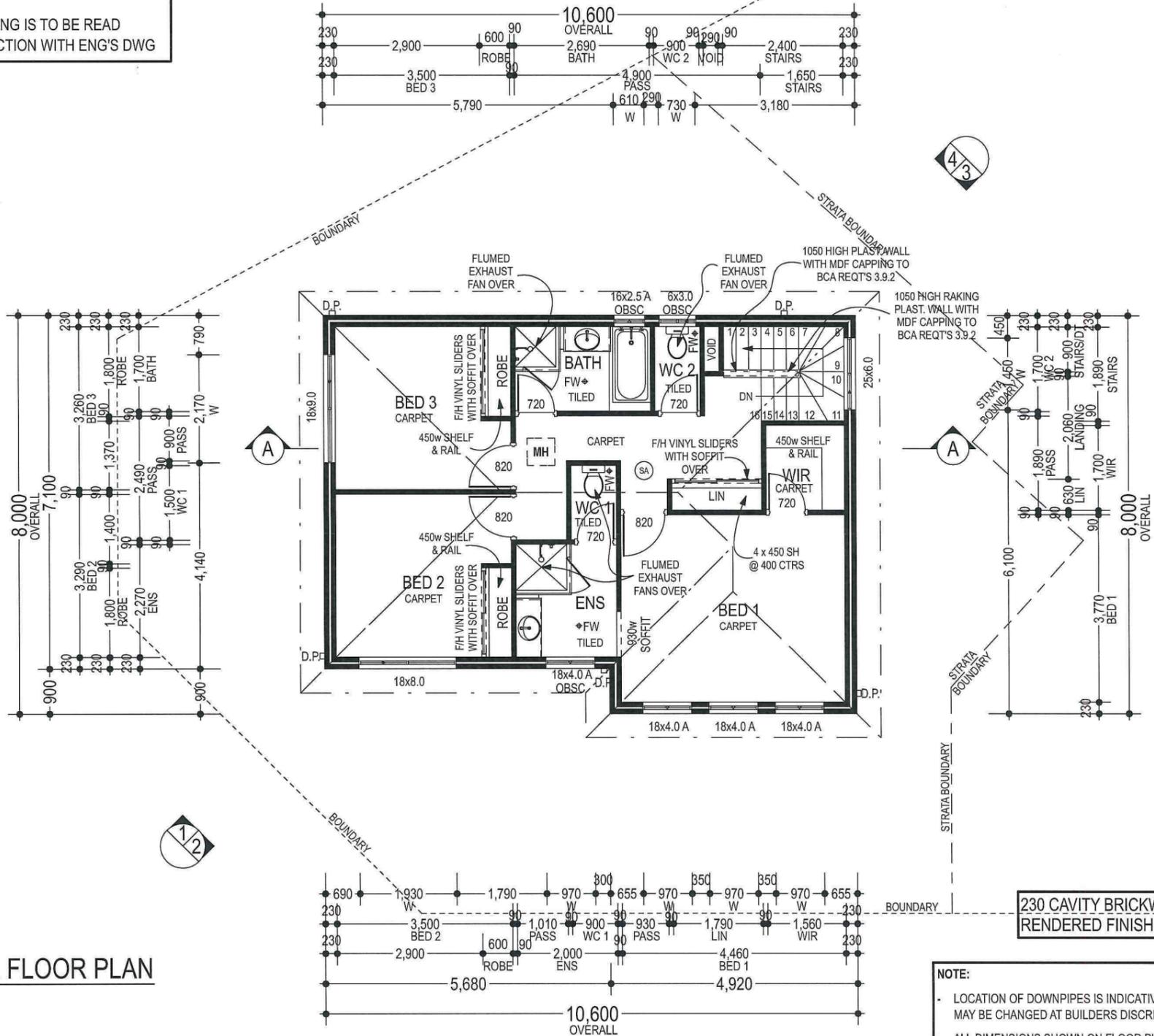
(SA) DENOTES SMOKE ALARM

PLUMBING NOTE:
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

NOTE:
R1.5 CAVITY INSULATION TO EXTERNAL WALLS.

NOTE:
THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG

AREAS: UPPER FLOOR		
	PERIM. (m)	AREA (m ²)
HOUSE AREA (Upper)	37.20	79.69
		79.69 m ²
UPPER ROOF AREAS		
Material Type	Area [m ² on the flat]	
Roof - Tiled	98.47	



UPPER FLOOR PLAN
SCALE 1:100

230 CAVITY BRICKWORK WITH RENDERED FINISH EXTERNALLY

NOTE:

- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- FLOORTECH FLOOR SLAB TO UPPER FLOOR TO ENGINEER'S DETAILS.
- PAINTED SAND RENDERED FINISH TO EXTERNAL WALLS UNLESS NOTED OTHERWISE.
- CL @ 28c + PLATE THROUGHOUT UPPER FLOOR UNLESS NOTED OTHERWISE.

NOTE:
ROOF CONSTRUCTION AS PER ENGINEER'S DETAIL.

DEPARTMENT OF PLANNING
17 OCT 2013
FILE 20-30458-1

Now LIVING
22 MOUNT STREET PERTH PH: 9261 1888
REG. BUILDER N°: 9769
A.B.N. 54 052 543 450
BGC RESIDENTIAL PTY LTD

REVISION	VO #	DRN	DATE	CHK
ENGINEERING		MKN	20.02.13	SZ
ELEC AMM.		JH	27.03.13	MKN
AMENDED		MC	01.05.13	JH
			19.08.13	MC

Sub-contractors to verify all dimensions on site.

CLIENT:
DATE:
CLIENT:
DATE:
BUILDER:
DATE:

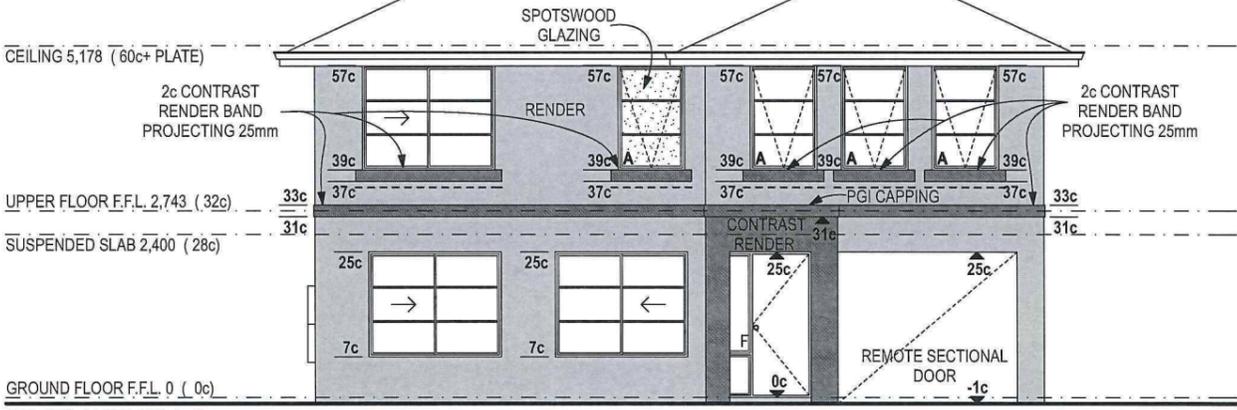
CLIENT: DOH
ADDRESS: LOT 4 (#10)
UNIT 1
VERMONT STREET
NOLLAMARA

© COPYRIGHT

ONE-OFF

MODEL N°	DATE
MAP REF.	WIND RATING
COASTAL CATEGORY	ENGINEERS DETAIL
HOME ID	SHEET N°
80773	19.08.13
	N1
	D10W
	2 OF 27

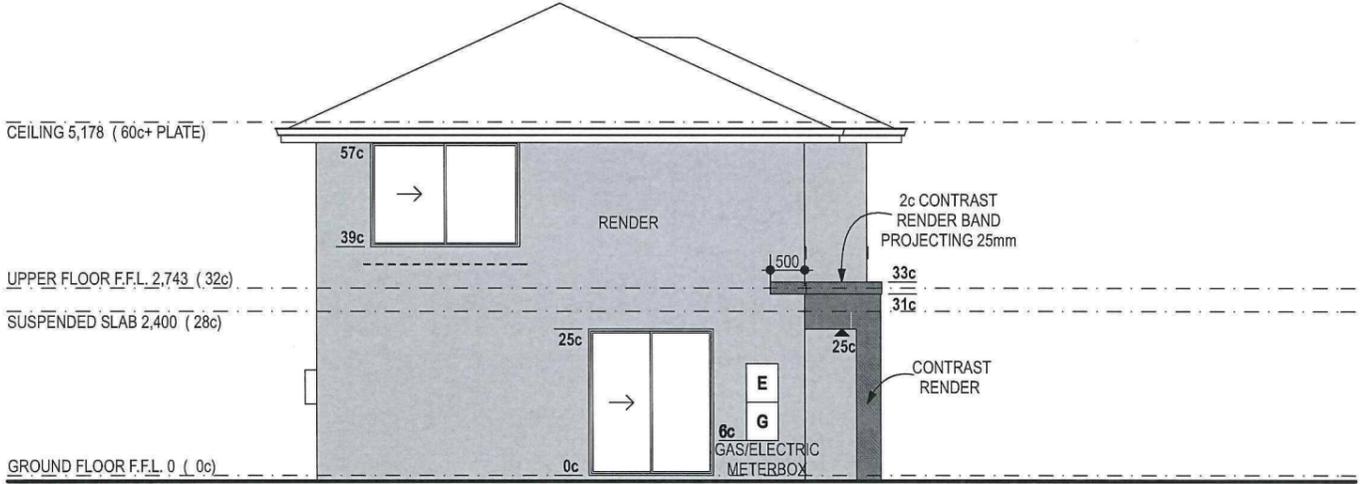
TILED ROOF ON 25°38'0"
(25°) PITCH.



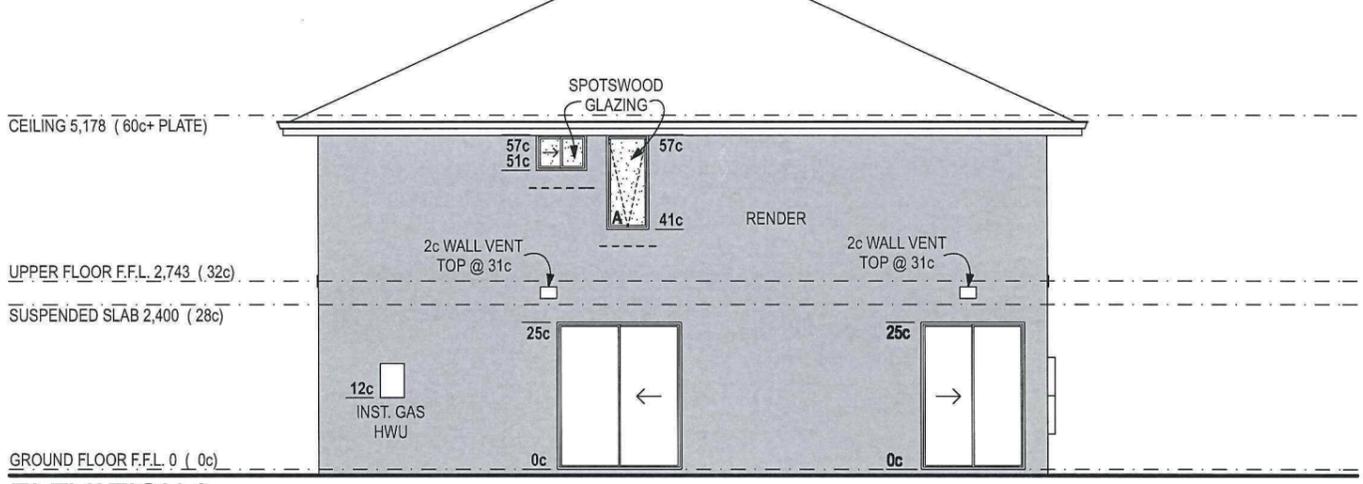
NOTE:

- EXTENT OF PAINTED SAND FINISH RENDER SHOWN SHADED, DARKER SHADE DENOTES CONTRAST RENDER.
- BUILDER RESERVES THE RIGHT TO FORM A "V-JOINT" TO EXTERNAL RENDERED WALLS AROUND UPPER FLOOR LEVELS.
- PROVIDE FLASHING TO ALL UPPER FLOOR WINDOWS AS SHOWN DASHED.
- FLOORTECH SUSPENDED FLOOR SLAB TO UPPER FLOOR TO ENG'S DETAILS.
- SELF-ADHESIVE SINGLE SIDED GLAZING BARS TO FRONT ELEVATION WINDOWS AS SHOWN.

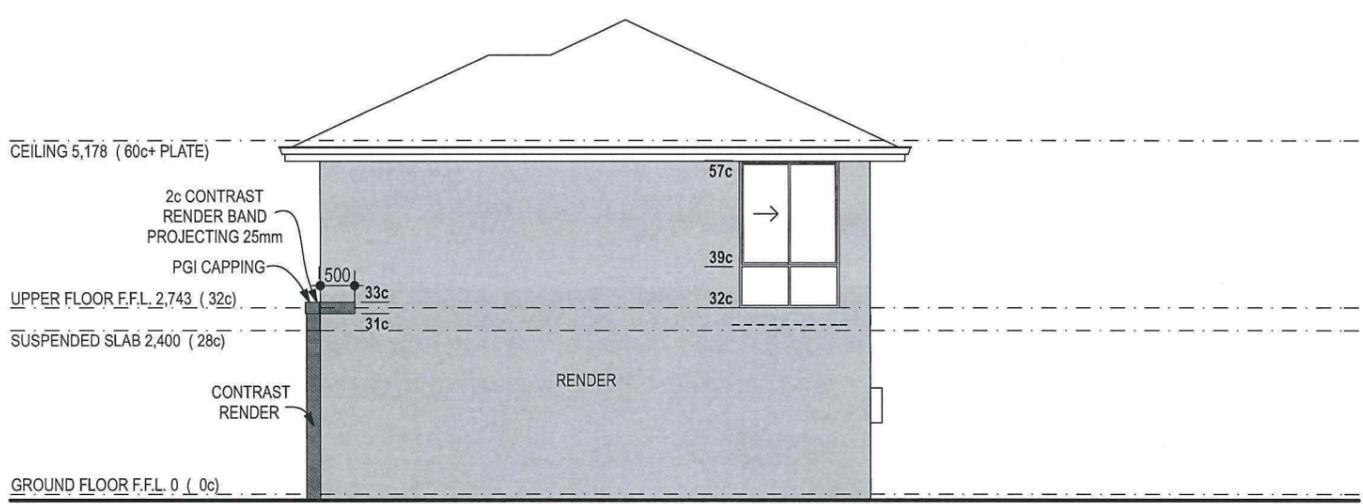
ELEVATION 1
SCALE 1:100



ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100



ELEVATION 4
SCALE 1:100

DEPARTMENT OF PLANNING
11 OCT 2013
FILE 20-SDU58-1



22 MOUNT STREET PERTH PH: 9261 1888
REG. BUILDER N°: 9769
A.B.N. 54 052 543 450
BGC RESIDENTIAL PTY LTD

REVISION	VO #	DRN	DATE	CHK
ENGINEERING		JE	20.02.13	SZ
ELEC AMM.		MKN	27.03.13	MKN
AMENDED		JH	01.05.13	JH
		MC	19.08.13	MC

CLIENT:
DATE:
CLIENT:
DATE:
BUILDER:
DATE:

CLIENT: DOH
ADDRESS: LOT 4 (#10)
UNIT 1
VERMONT STREET
NOLLAMARA

ONE-OFF	
MODEL N°	DATE
MAP REF.	WIND RATING
COASTAL CATEGORY	ENGINEERS DETAIL
HOME ID	SHEET N°
80773	3 OF 27

NOTE:

- CLAY BRICKS TO ALL INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE & GARAGE.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY

(SA) DENOTES SMOKE ALARM

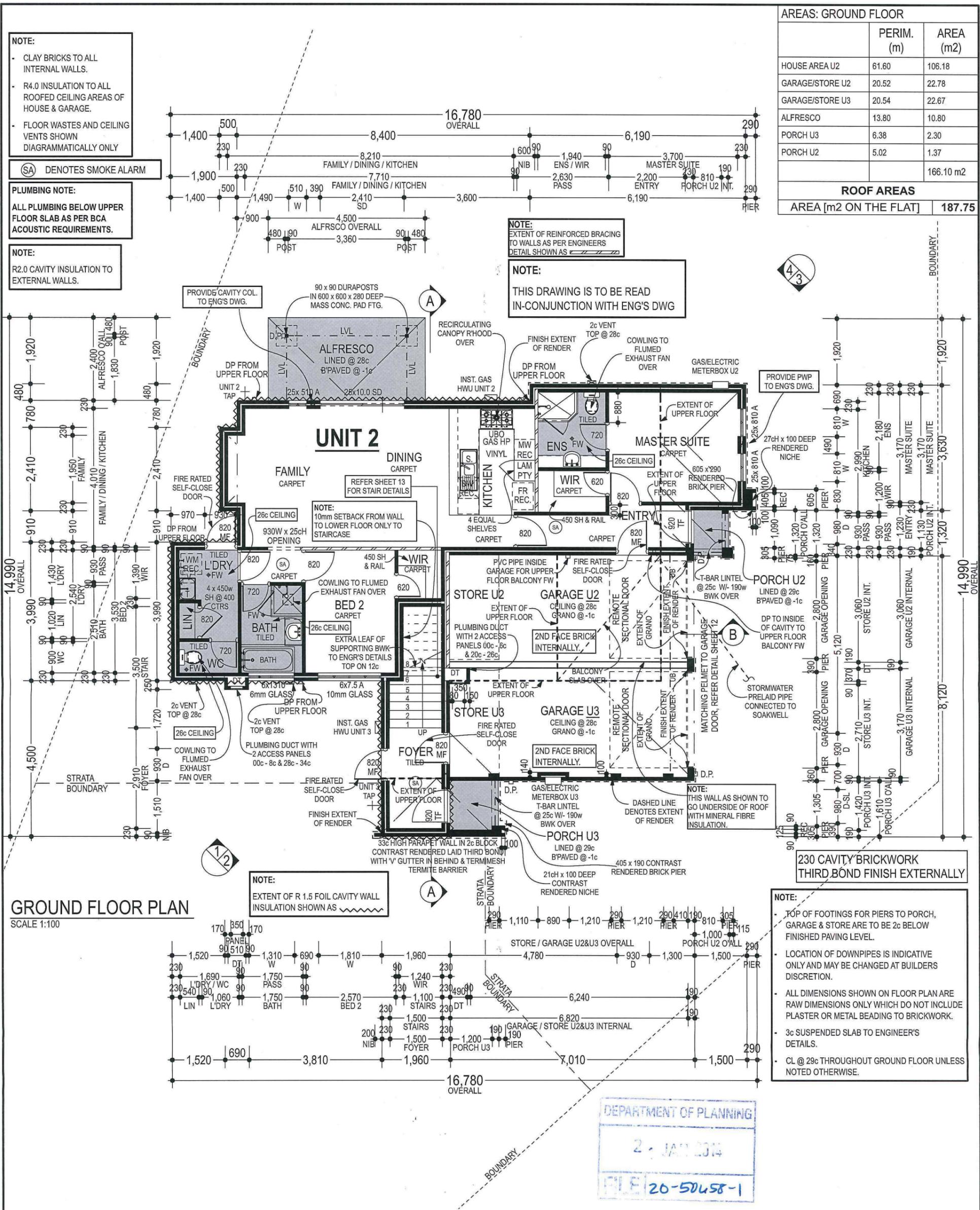
PLUMBING NOTE:

ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

NOTE:

R2.0 CAVITY INSULATION TO EXTERNAL WALLS.

AREAS: GROUND FLOOR		
	PERIM. (m)	AREA (m2)
HOUSE AREA U2	61.60	106.18
GARAGE/STORE U2	20.52	22.78
GARAGE/STORE U3	20.54	22.67
ALFRESCO	13.80	10.80
PORCH U3	6.38	2.30
PORCH U2	5.02	1.37
		166.10 m2
ROOF AREAS		
AREA [m2 ON THE FLAT]		187.75



DEPARTMENT OF PLANNING
2 JAN 2014
FILE 20-50458-1

NOW living

LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENGINEERING		MKN	20.02.13	SZ
ELEC AMM.		JH	28.03.13	MKN
AMENDED		MC	01.05.13	JH
AMENDED		MC	18.08.13	MC
AMENDED		MC	20.01.14	MC

CLIENT:

DATE:

CLIENT:

DATE:

BUILDER:

DATE:

CLIENT: D. O. H.

ADDRESS: LOT 4 (#10)
UNIT 2 & 3
VERMONT STREET
NOLLAMARA

© COPYRIGHT

ONE-OFF

MODEL N°	DATE
TBA	20.01.14
MAP REF.	WIND RATING
TBA	N1
COASTAL CATEGORY	ENGINEERS DETAIL
TBA	D10W
HOME ID	SHEET N°
81064	9 OF 27

AREAS: UPPER FLOOR		
	PERIM. (m)	AREA (m ²)
HOUSE AREA U3	50.60	102.11
BALCONY	13.18	10.75
		112.86 m ²
ROOF AREAS		
AREA [m ² ON THE FLAT]		187.75

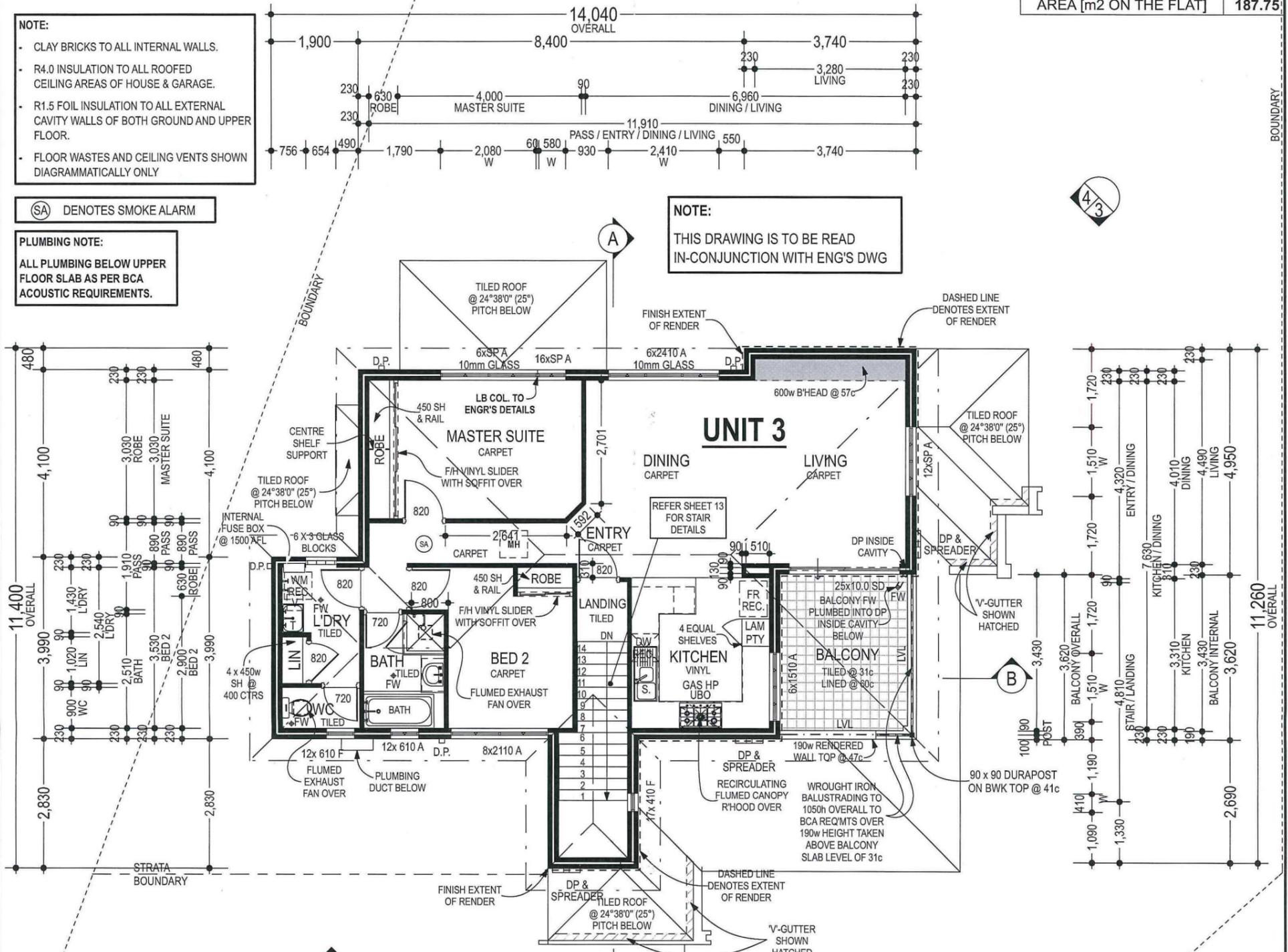
NOTE:

- CLAY BRICKS TO ALL INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE & GARAGE.
- R1.5 FOIL INSULATION TO ALL EXTERNAL CAVITY WALLS OF BOTH GROUND AND UPPER FLOOR.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY

(SA) DENOTES SMOKE ALARM

PLUMBING NOTE:
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

NOTE:
THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG



UPPER FLOOR PLAN
SCALE 1:100

230 CAVITY BRICKWORK
THIRD BOND FINISH EXTERNALLY

NOTE:

- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 28 + PL THROUGHOUT UPPER FLOOR UNLESS NOTED OTHERWISE.

DEPARTMENT OF PLANNING
2 JAN 2014
FILE 20-SD458-1.

NOW living
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TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N^o: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENGINEERING		DA	20.02.13	SZ
ELEC AMM.		MKN	28.03.13	MKN
AMENDED		JH	01.05.13	JH
AMENDED		MC	18.08.13	MC
		MC	20.01.14	MC

CLIENT:
DATE:
CLIENT:
DATE:
BUILDER:
DATE:

CLIENT:
D. O. H.
ADDRESS:
LOT 4 (#10)
UNIT 2 & 3
VERMONT STREET
NOLLAMARA

© COPYRIGHT

ONE-OFF

MODEL N ^o	DATE
TBA	20.01.14
MAP REF.	WIND RATING
TBA	N1
COASTAL CATEGORY	ENGINEERS DETAIL
TBA	D10W
HOME ID	SHEET N ^o
81064	10 OF 27

NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

(SA) DENOTES SMOKE ALARM

PLUMBING NOTE:

ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

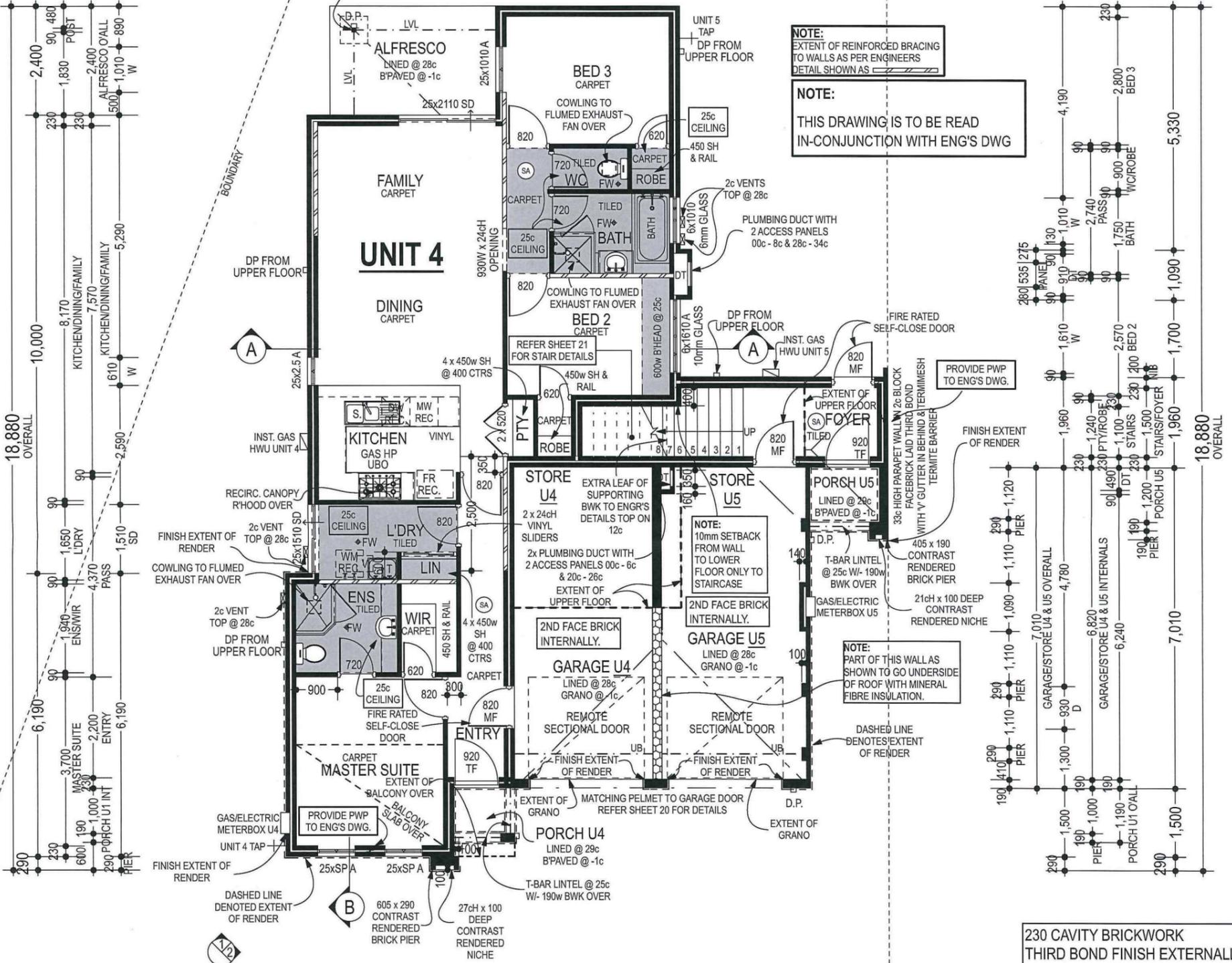
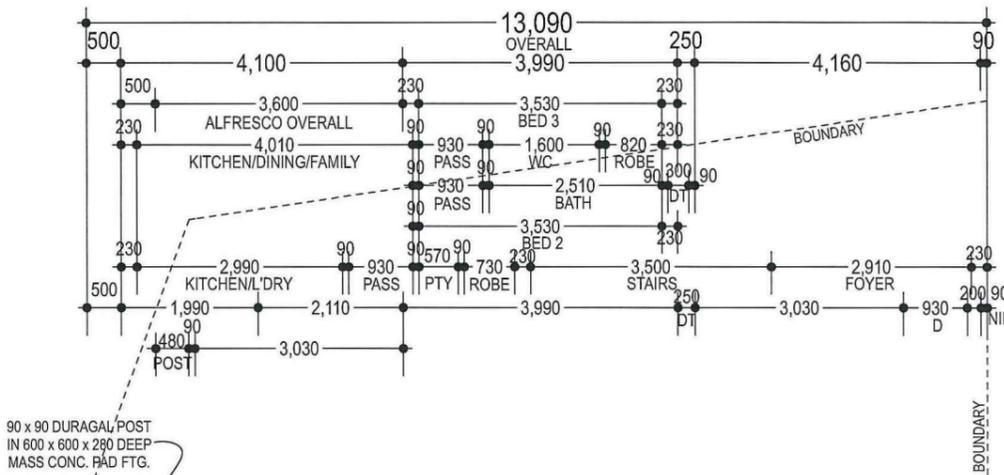
NOTE:

ROOF CONSTRUCTION AS PER ENGINEER'S DETAIL.

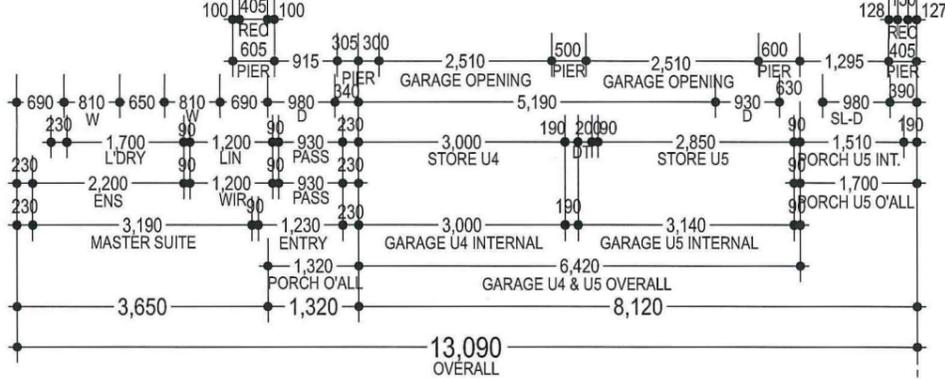
NOTE:

R2.0 CAVITY INSULATION TO EXTERNAL WALLS.

AREAS: GROUND FLOOR		
	PERIM. (m)	AREA (m2)
HOUSE AREA (Ground)	63.86	119.95
GARAGE U5	17.80	18.44
GARAGE U4	17.24	16.86
ALFRESCO	12.00	8.64
STORE U5	9.34	4.56
STORE U4	8.78	4.17
PORCH U5	6.84	2.63
PORCH U4	7.23	1.78
		177.03 m2
GROUND ROOF AREAS		
Material Type	Area [m2 on the flat]	
Roof - Tiled	47.73	



GROUND FLOOR PLAN
SCALE 1:100



230 CAVITY BRICKWORK
THIRD BOND FINISH EXTERNALLY

NOTE:

- TOP OF FOOTINGS FOR PIERS TO PORCH, GARAGE & STORE ARE TO BE 2c BELOW FINISHED PAVING LEVEL.
- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 29c THROUGHOUT GROUND FLOOR UNLESS NOTED OTHERWISE.

NOW living

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TEL: (08) 6461 5350 FAX: (08) 6461 5383
P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 009 063076

REVISION	VO #	DRN	DATE	CHK
ENGINEERING		DA	20.02.13	SZ
ELEC AMM.		MKN	27.03.13	MKN
AMENDED		JH	01.05.13	JH
AMENDED		MC	19.08.13	MC
AMENDED		MC	16.12.13	MC

CLIENT: DEPARTMENT OF PLANNING
DATE: 17 DEC 2013
CLIENT: FILE 20-5DU 58-1
DATE: _____
BUILDER: _____
DATE: _____

CLIENT: D.O.H
ADDRESS: LOT 4 (#10)
UNIT 4 & 5
VERMONT STREET
NOLLAMARA

ONE-OFF	
MODEL N°	DATE
T.B.A.	16.12.13
MAP REF.	WIND RATING
T.B.A.	N1
COASTAL CATEGORY	ENGINEERS DETAIL
T.B.A.	D10W
HOME ID	SHEET N°
81065	17 OF 27

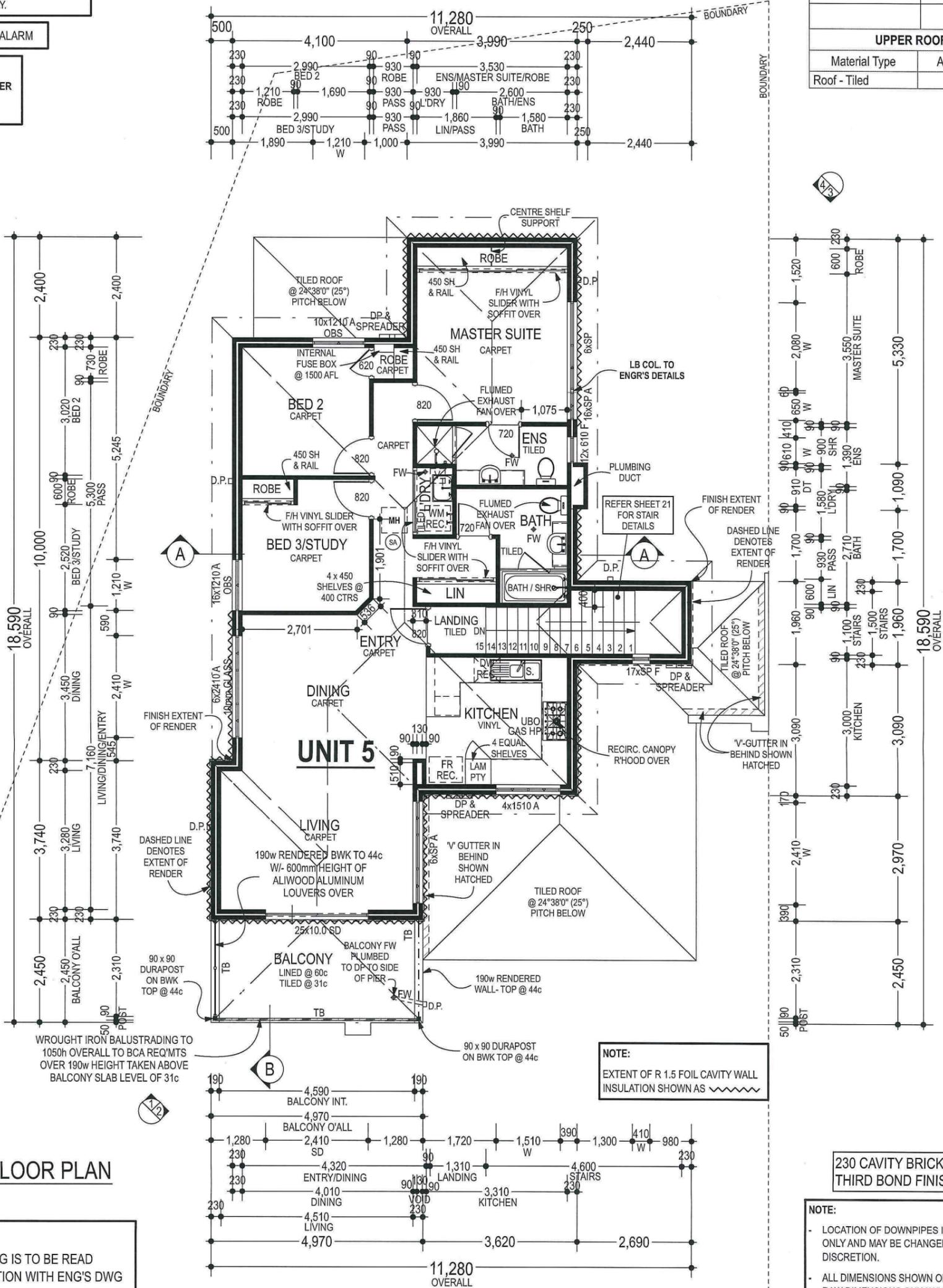
NOTE:

- CLAY BRICKS TO INTERNAL WALLS.
- R4.0 INSULATION TO ALL ROOFED CEILING AREAS OF HOUSE ONLY.
- FLOOR WASTES AND CEILING VENTS SHOWN DIAGRAMMATICALLY ONLY.

(SA) DENOTES SMOKE ALARM

PLUMBING NOTE:
ALL PLUMBING BELOW UPPER FLOOR SLAB AS PER BCA ACOUSTIC REQUIREMENTS.

AREAS: UPPER FLOOR		
	PERIM. (m)	AREA (m ²)
HOUSE AREA (Upper)	55.34	117.40
BALCONY	14.84	12.18
		129.58 m ²
UPPER ROOF AREAS		
Material Type	Area [m ² on the flat]	
Roof - Tiled	150.47	



UPPER FLOOR PLAN
SCALE 1:100

NOTE:
THIS DRAWING IS TO BE READ IN-CONJUNCTION WITH ENG'S DWG

230 CAVITY BRICKWORK THIRD BOND FINISH EXTERNALLY

NOTE:

- LOCATION OF DOWNPIPES IS INDICATIVE ONLY AND MAY BE CHANGED AT BUILDERS DISCRETION.
- ALL DIMENSIONS SHOWN ON FLOOR PLAN ARE RAW DIMENSIONS ONLY WHICH DO NOT INCLUDE PLASTER OR METAL BEADING TO BRICKWORK.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- CL @ 28c + PL THROUGHOUT UPPER FLOOR UNLESS NOTED OTHERWISE.

NOW living

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REG. BUILDER N°: 6915 A.C.N. 009 063076

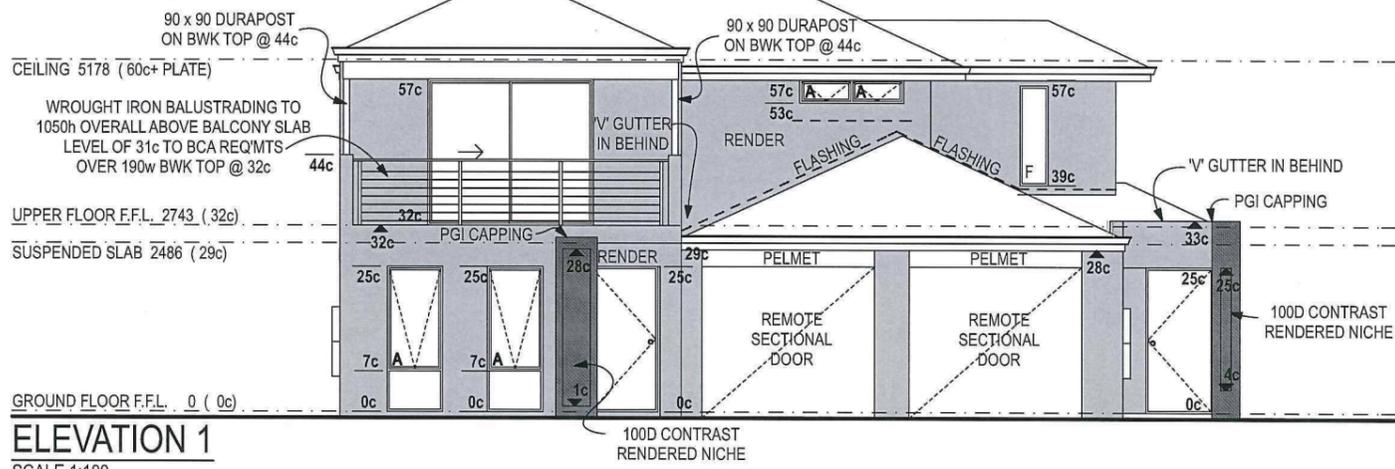
REVISION	VO #	DRN	DATE	CHK
ENGINEERING		MKN	27.03.13	MKN
ELEC AMM.		JH	01.05.13	JH
AMENDED		MC	19.08.13	MC
AMENDED		MC	16.12.13	MC

CLIENT: D.O.H
DATE: 17 DEC 2013
CLIENT: DEPARTMENT OF PLANNING
DATE: 17 DEC 2013
BUILDER: FILE 20-50458-1
DATE:

CLIENT: D.O.H
ADDRESS: LOT 4 (#10)
UNIT 4 & 5
VERMONT STREET
NOLLAMARA

ONE-OFF	
MODEL N°	DATE
MAP REF.	WIND RATING
COASTAL CATEGORY	ENGINEERS DETAIL
HOME ID	SHEET N°
T.B.A.	16.12.13
T.B.A.	N1
T.B.A.	D10W
81065	18 OF 27

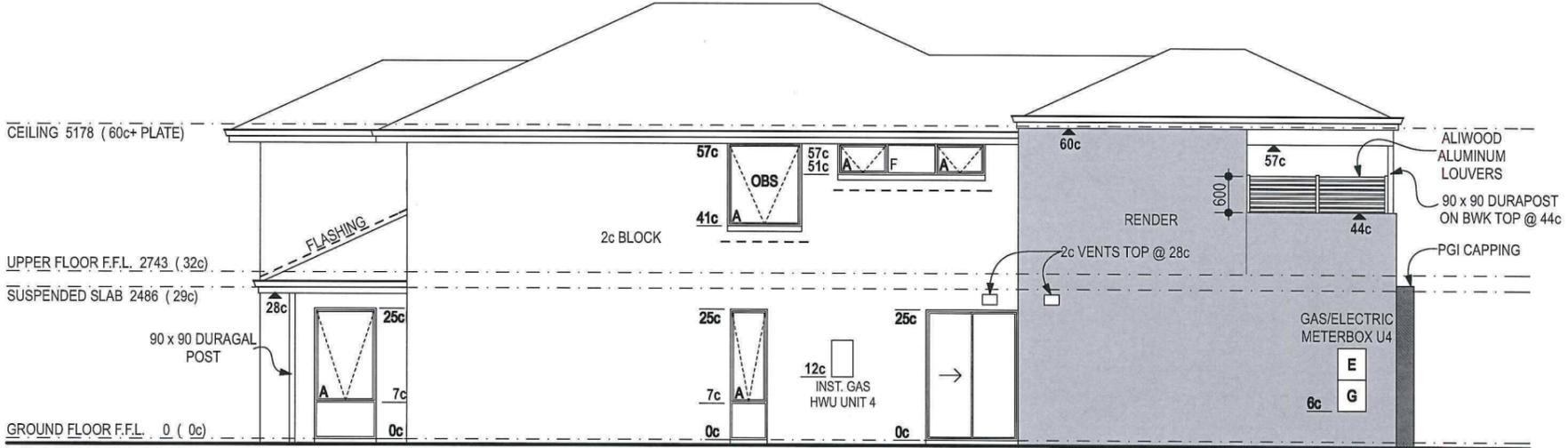
TILED ROOF ON
25°38'0" (25°) PITCH.



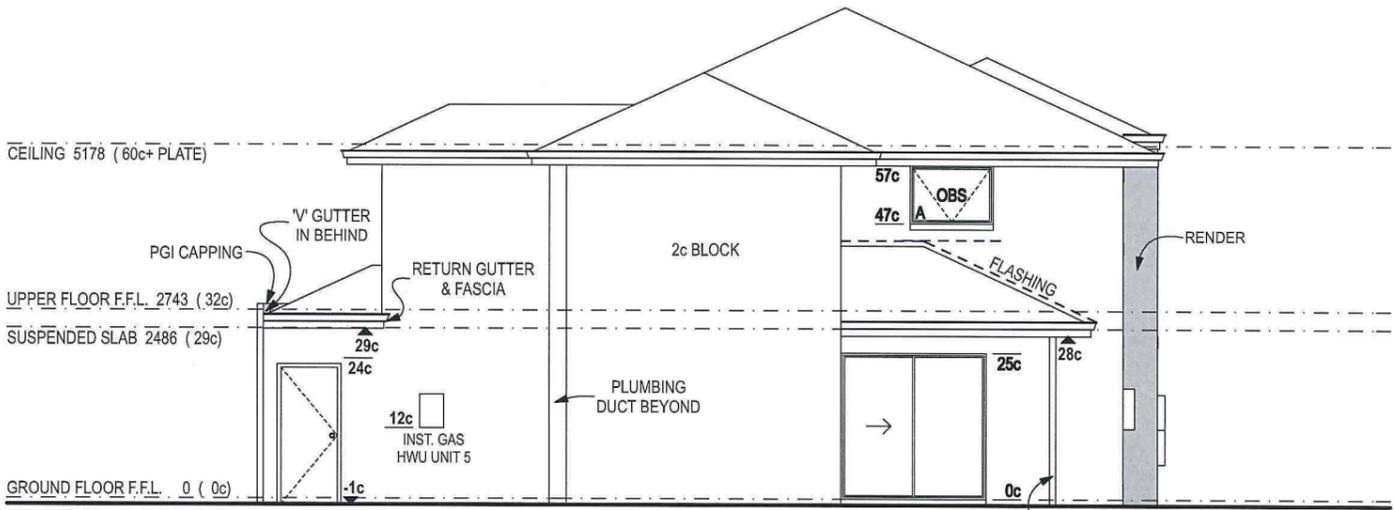
NOTE:

- EXTENT OF PAINTED SAND FINISH RENDER SHOWN SHADED, DARKER SHADE DENOTES CONTRAST RENDER.
- 3c SUSPENDED SLAB TO ENGINEER'S DETAILS.
- THE BUILDER RESERVES THE RIGHT TO FORM A V' JOINT TO EXTERNAL WALLS AROUND UPPER FLOOR LEVELS.
- PROVIDE FLASHING TO ALL UPPER FLOOR WINDOWS AS INDICATED DASHED LINE

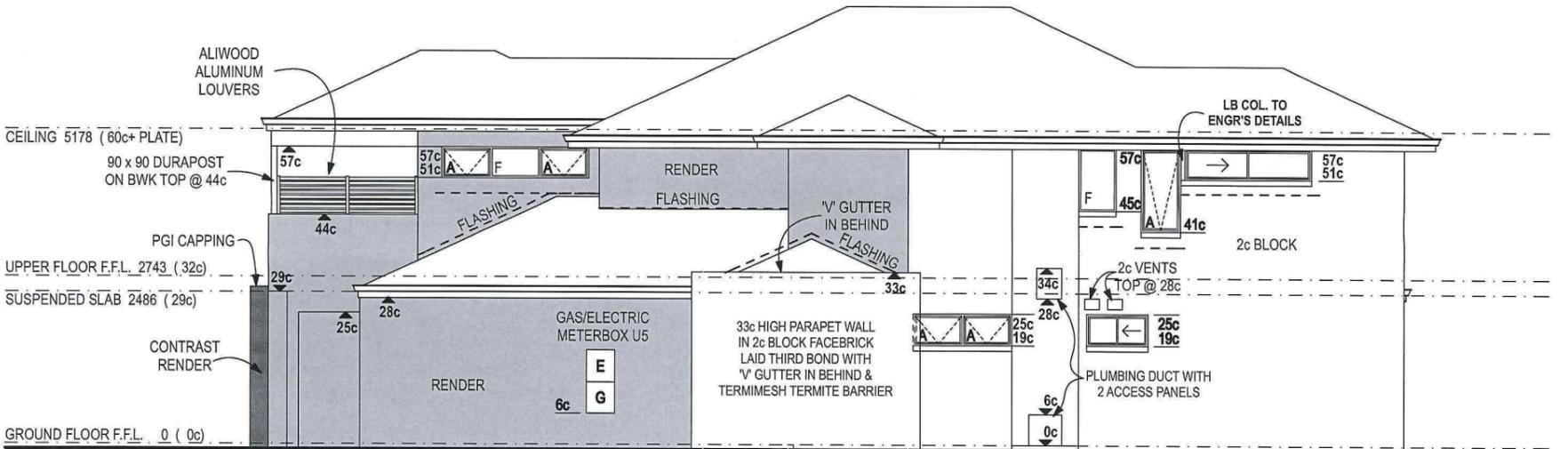
ELEVATION 1
SCALE 1:100



ELEVATION 2
SCALE 1:100



ELEVATION 3
SCALE 1:100



ELEVATION 4
SCALE 1:100

NOW living

LEVEL 3, 6 BENNETT STREET, EAST PERTH, WA 6004
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P.O. BOX 131, MT. HAWTHORN WA 6915
REG. BUILDER N°: 6915 A.C.N. 009 063076

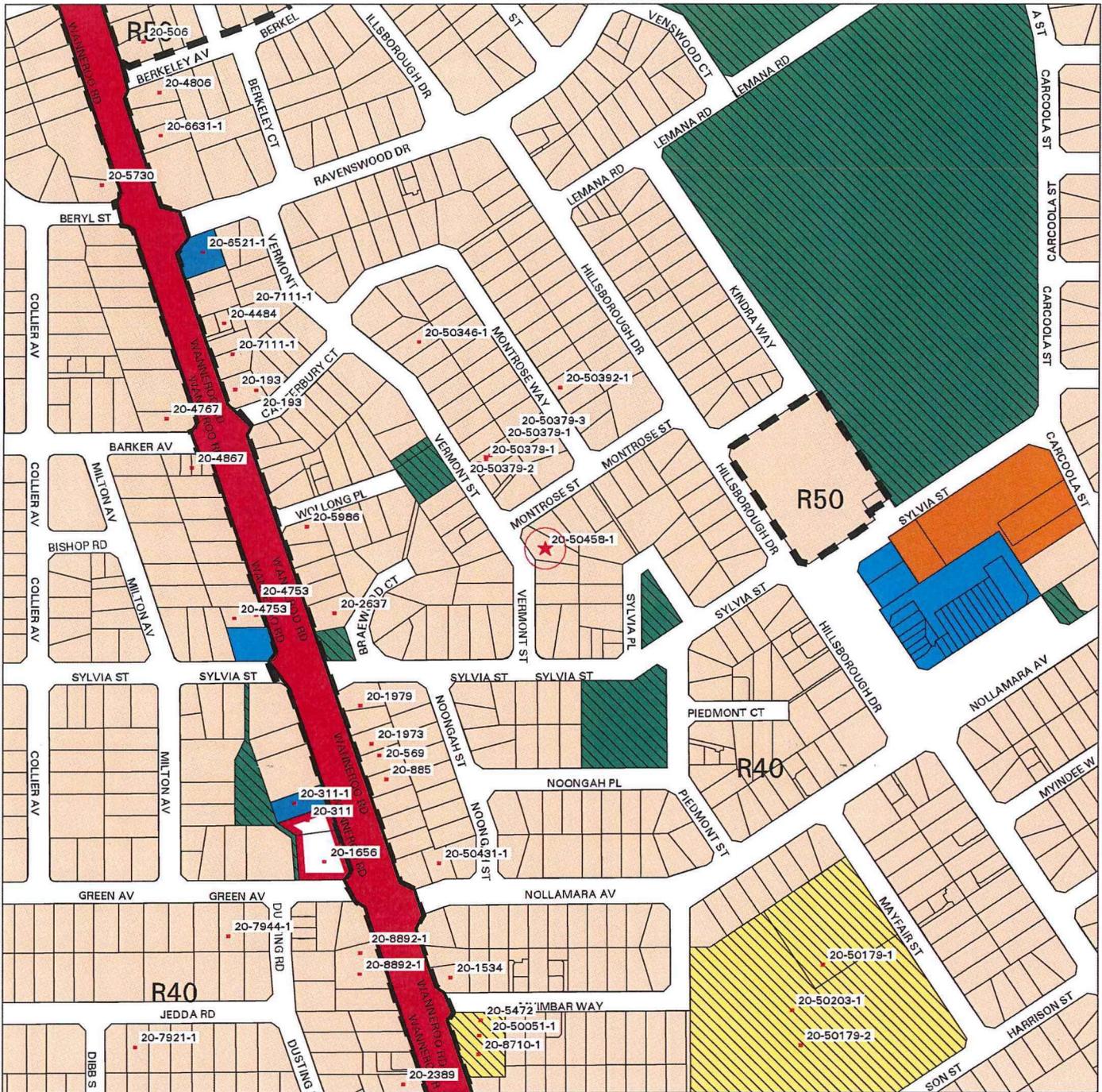
REVISION	VO #	DRN	DATE	CHK
ENGINEERING		DA	20.02.13	SZ
ELEC AMM.		MKN	27.03.13	MKN
AMENDED		JH	01.05.13	JH
AMENDED		MC	19.08.13	MC
AMENDED		MC	16.12.13	MC

Sub-contractors to verify all dimensions on site.

CLIENT: DEPARTMENT OF PLANNING
DATE: 17 DEC 2013
CLIENT: FILE 20-50458-1
DATE: 16.12.13

CLIENT: D.O.H
ADDRESS: LOT 4 (#10)
UNIT 4 & 5
VERMONT STREET
NOLLAMARA

ONE-OFF	
MODEL N°	DATE
	16.12.13
MAP REF.	WIND RATING
T.B.A.	N1
COASTAL CATEGORY	ENGINEERS DETAIL
T.B.A.	D10W
HOME ID	SHEET N°
81065	19 OF 27

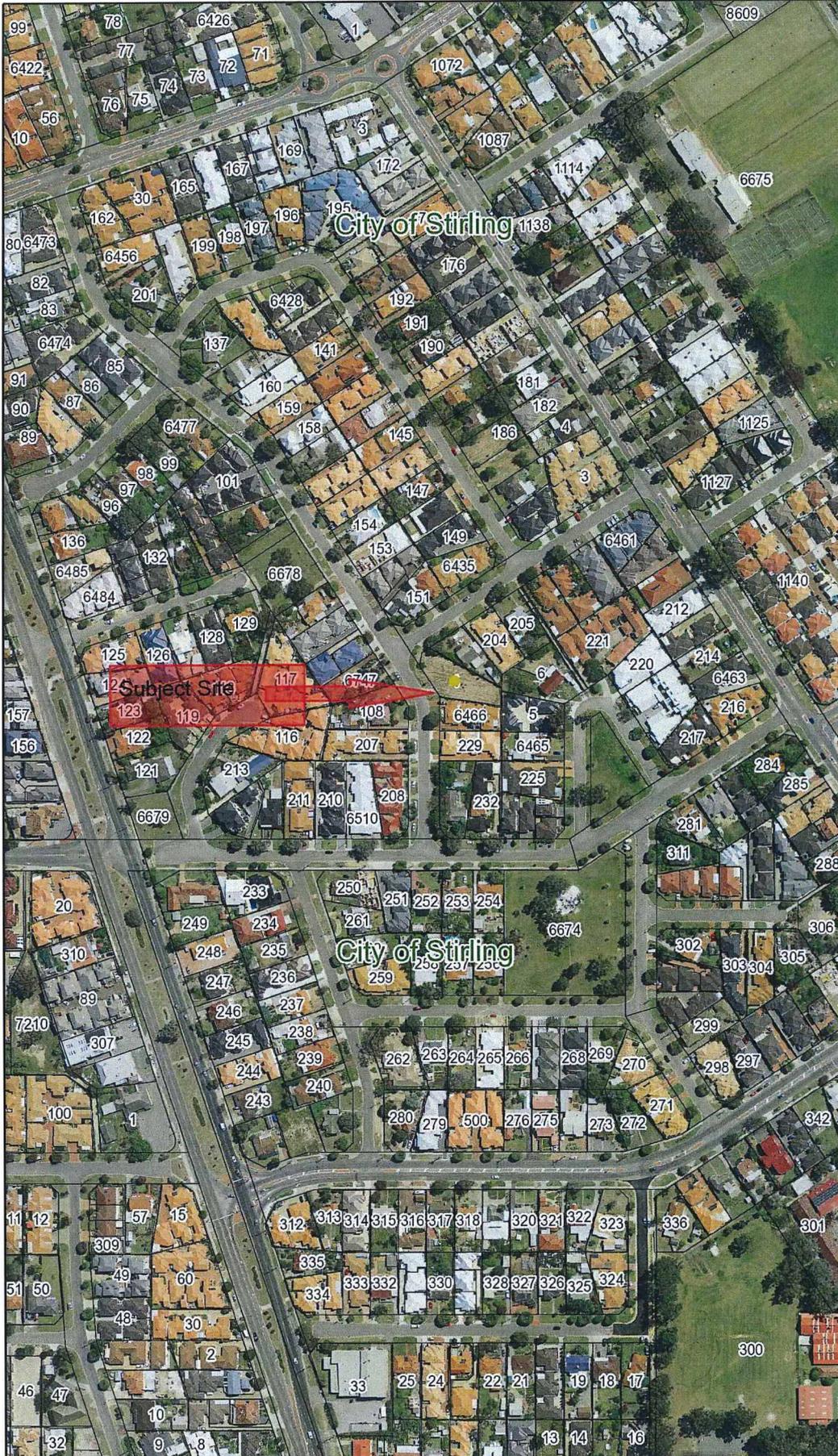


Development Application 20-50458-1 (MGA ref 390015mE 6472236mN Zone 50)

This data is to be used for the processing of subdivision applications only.

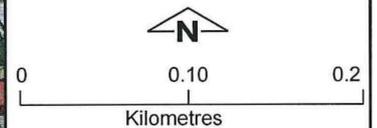
-  APPROX LOCATION OF APPLICATION
-  PRIMARY REGIONAL ROADS
-  CADASTRAL BOUNDARY
-  DEVELOPMENT APPLICATION
-  R CODE BOUNDARY
-  RESIDENTIAL
-  PUBLIC OPEN SPACE
-  BUSINESS
-  CIVIC
-  LOCAL CENTRE
-  SPECIAL USE
-  PUBLIC USE PRIMARY SCHOOL
-  PUBLIC USE UTILITY

141925d.eps



Legend

- Local Government Areas
- Cadastre (view 1)
- Cadastre (view 2)
- Cadastre (view 3)
- Cadastre (view 4)
- W.A. Coastline



1: 4,443
at A4

Projection:
WGS 1984 Web Mercator Auxiliary Sphe

Notes:
The data that appears on the map may be out of date, not intended to be used at the scale displayed, or subject to license agreements. This information is stored in the relevant layers metadata. For these reasons the map should not be distributed outside of the Department.
Map was produced using DoP's PlanViewWA.

PlanViewWA Map

INTERNAL USE ONLY

Internal Spatial Viewer
[Link to viewer](#)

Produced by:
Date produced: 13-Mar-2014



ITEM NO: 9.5

PROPOSED MOBILE ASPHALT PROCESSING PLANT

WAPC OR COMMITTEE:	Statutory Planning Committee
REPORTING AGENCY:	Department of Planning
REPORTING OFFICER:	Director, Metropolitan Planning South East, Perth and Peel
AUTHORISING OFFICER:	Director, Metropolitan Planning South East, Perth and Peel
AGENDA PART:	G
FILE NO:	25-4116-3
DATE:	21 March 2014
ATTACHMENT(S):	Attachment 1 - Location Plan Attachment 2 - MRS Zoning Attachment 3 - Site Plan Attachment 4 - Photos (Pit 2 & Quarry Base) Attachment 5 - City of Gosnells Officer's Report Attachment 6 - Council's Schedule of Submissions Attachment 7 - Additional Submissions
REGION SCHEME ZONING:	Parks and Recreation
LOCAL GOVERNMENT:	City of Gosnells
LOCAL SCHEME ZONING:	Parks and Recreation
LGA RECOMMENDATION(S):	N/A
REGION DESCRIPTOR:	
RECEIPT DATE:	9 January 2014 (valid)
PROCESS DAYS:	84 days
APPLICATION TYPE:	Development
CADASTRAL REFERENCE:	Lot 467 Stephen Street, Orange Grove

RECOMMENDATION:

That the Western Australian Planning Commission resolves to approve a temporary Mobile Asphalt Processing Plant at Lot 467 Stephen Street, Orange Grove subject to the following conditions:

Conditions:

1. *The development hereby granted is for a limited period of 3 years, after which the development is to be removed and the site reinstated.*

2. *The Mobile Asphalt Plant shall not be operated outside the hours of 5am to 5pm weekdays or 5am to 4pm Saturdays.*
3. *No cartage of asphalt from the site shall occur outside the hours of 6am to 6pm weekdays or 6am to 4pm Saturdays.*

SUMMARY:

Boral Resources has lodged an application for operation of a temporary Mobile Asphalt Plant (MAP) within the existing Boral hard rock quarry at Orange Grove, Gosnells to service road works under the Gateway WA Project.

Significant public opposition has been raised to the MAP in the context of nearby sensitive land uses and potential adverse impacts upon the amenity, health, traffic and safety of residents in the area.

Based upon technical reports provided as part of the application, the proposal will operate within acceptable standards and resulting truck movements are shown to be only slightly above those of the existing quarry operations.

The proposal is subject to separate environmental approval requirements under Part 5 of the *Environmental Protection Act 1986* which will be required to verify acceptability of the operations from an environmental, health and safety perspective.

LEGISLATION / STRATEGIC PLAN / POLICY:

Legislation Planning and Development Act 2005
Section: Part 10

Strategic Plan
Strategic Goal: Strategic Goal 2: Planning
Outcomes: Effective Delivery of Integrated Plans
Strategies: Implement State and Regional Planning priorities

Policy
Number and / or Name: *Development Control Policy 1.2 - Development Control - General Principles.*

Development Control Policy 5.3 - Use of Land Reserved for Parks and Recreation and Regional Open Space

State Planning Policy No. 2.4 - Basic Raw Materials

State Planning Policy 4.1 - State Industrial Buffer Policy

Environmental Protection Authority Guidance Note 3 - Separation Distances between Industrial and Sensitive Land Uses

Policy
Compliance: Proposal complies with WAPC policies unless discussed in Planning Assessment section.

INTRODUCTION:

The application seeks approval for installation of a temporary mobile asphalt plant (MAP) within the Boral Resources (WA) hard rock quarry operations at Orange Grove. The MAP is intended to supply asphalt for the Gateway WA project, incorporating a major upgrade of the road network surrounding Perth Airport and the freight and industrial centres of Kewdale and Forrestfield. Following completion of the Gateway Project the plant is to be decommissioned and relocated.

The MAP, an Astec T400 Double Barrel Continuous Dryer/Mixer Plant, is anticipated to produce up to 500,000 tonnes of asphalt over the life of the project. It is proposed to be installed at the base of the Stage Two Quarry pit, located within Lot 467 Stephen Street, Orange Grove. The MAP will utilise aggregate and sand materials extracted and processed on site.

The MAP comprises various elements, including raw material storage bays, hot bitumen kettles, bins, silos, conveyors, dryer/ mixer drum, baghouse and stack system, a weighbridge and laboratory. A diesel generator will be used to supply power while burner fuel will be used to heat the double barrel drying drum. A typical layout of the MAP is provided at **Attachment 3**. The approximate cost of the development is \$2.2 million.

Operating hours are proposed to be from 5am to 5pm on weekdays, with transport occurring between 6am and 6pm. On Saturdays the operating hours are proposed to be from 5am to 4pm, with transport occurring between 6am and 4pm. Although it was originally intended to accommodate occasional out of hours operation, Boral has advised that this will no longer be required.

The MAP will be serviced by a regular workforce of approximately 10 to 14, comprising 6 to 8 truck drivers working any one shift. Additional subcontractors may also be employed from time to time as part of general maintenance requirements.

The proposal has been forwarded to the Commission for determination under Clause 18 of the Metropolitan Region Scheme (MRS) by reason that the development is located on land which is reserved for Parks and Recreation. Clause 30(1) of the MRS sets out the following factors which must be taken into account when determining a development application:

- the purpose for which the land is zoned or reserved under the MRS;
 - the orderly and proper planning of the locality; and
 - the preservation of the amenities of the locality.
-

CONSULTATION:

City of Gosnells

The City of Gosnells has not provided comments on the proposed development within the 42 day time period stipulated under Clause 29(3) of the Metropolitan Region Scheme.

The matter is due to be considered at the Ordinary Council meeting scheduled for 7:30pm on 25 March 2014. A copy of the officer's report has been sourced from the Council Agenda made available on 20 March 2014. The report outlines considerations relevant to assessment of the proposal and suggests matters that the Commission should take into account as part of its determination. The report raises no significant planning objections to the operation of the MAP. A full copy of the report is provided at **Attachment 5**.

Department of Mines and Petroleum (DMP)

DMP advises of no objection.

Department of Parks and Wildlife (DPaW)

DPaW notes that the proposal is to be located within an existing quarry and will not likely have any environmental impacts on the surrounding vegetation. DPaW therefore advises of no comments on the proposal.

Department of Health (DOH)

DOH advises that the International Agency for Research on Cancer classifies asphalt fumes as a possible human carcinogen. Fume emissions from the asphalt plant can adhere to dust from the quarry and travel long distances.

Consequently DOH would require Boral Resources (WA) to manage both dust from the quarry and fume emissions from the asphalt plant so that PM₁₀ (particulate matter of 10 microns & smaller) concentrations do not exceed 50 ug/m³ at the quarry buffer.

DOH does not accept 'visible dust crossing the boundary' as a suitable measure of dust exceeding the prescribed levels.

Boral has sought to qualify DOH requirements in relation to the operation of the MAP. Although both parties agree that it is unlikely for adherence of emissions and dust to occur outside of the MAP itself, there is still a need for Boral to actively manage dust from the quarry to ensure that local residents are not affected.

Department of Environment Regulation (DER)

DER (Contaminated Sites Branch) advises that, as the proposal is not for a more sensitive use, based on available information and provided appropriate environmental management procedures are adhered to it raises no objection to the development of a mobile asphalt plant within the existing quarry.

DER (Landuse Planning) advises that it has no comments to make in general with regard to the additional land use. DER is of the opinion that this additional land use can be managed satisfactorily as part of DER's Part V responsibilities under the *Environmental Protection Act (1986)* and *Environmental Protection Regulations 1987*.

Although a works approval application for the MAP is currently being assessed by DER, it will not be granted until planning approval has been issued. DER has referred the works approval to Department of Health for its advice.

Department of Fire and Emergency Services (DFES) Unexploded Ordnance Services (UXO)

DFES advises that the risk of UXO on the site can be regarded as minimal. This is on the basis that the processing plant is to be constructed in the base of a very large and deep quarry which has been excavated many metres below what would have been the original ground level present during the Military training period of the early 1900's.

Public Consultation

The City of Gosnells undertook public consultation on the proposal for a period of 21 days, closing on 24 January 2014.

A total of 99 submissions were received all of which objected to the proposal. Council's schedule of submissions, is provided at **Attachment 6**. The primary matters raised in the submissions have been addressed in detail as part of the City of Gosnells officer's assessment report.

The Western Australian Planning Commission has received several submissions which are identical to those provided to the City of Gosnells. A further two submissions, from the same submitter, outline concerns regarding the accuracy of modelling methodology within the air assessment report and requests that the proposal be formally referred to the Environmental Protection Authority (**Attachment 7**).

The Office of the Environmental Protection Authority (OEPA) has reviewed the proposal following preliminary correspondence from Boral and referrals from the local community. The OEPA considers the proposal not to be of such significance as to warrant consideration of a formal level of assessment and that it can be appropriately managed under existing conditions and commitments applicable to the existing quarry operations, and amendments to the Licence being considered by DER.

Planning assessment:***Purpose for which the land is zoned/ reserved under the MRS***

Lot 467 is partially zoned 'Rural' and partially reserved for 'Parks and Recreation' (P&R) under the Metropolitan Region Scheme (**Attachment 2**). The MAP is proposed to be located within the P&R component.

The P&R reserve applying to Lot 467 and adjoining landholdings would appear to be aligned with the significant landscape feature of the Darling Escarpment and is contiguous with the reservation applying to the Banyowla Regional Park to the east.

A P&R reservation is usually applied to land that is considered to have regional significance for ecological, recreation or landscape purposes. The Commission's *Development Control Policy 5.3 - Use of Land Reserved for Parks and Recreation and Regional Open Space* provides guidance on the Commission's consideration of applications for the use or development of land reserved for P&R. This Policy outlines a presumption against development that is inconsistent with the purpose of the P&R reservation.

Although the intent is to preserve P&R land for recreational and/or conservation purposes, each application must be considered on its merits based upon a range of factors.

The subject land forms part of an established quarry which has existed at this location, albeit at a lesser scale, since 1962. Examination of historic mapping indicates that the P&R reservation over Lot 467 has remained unchanged since adoption of the Metropolitan Region Scheme in 1963.

Approvals for expansion of the Boral Quarry have been granted by the Commission in recognition of the significance of the raw material extraction for development of the Perth metropolitan area. This approach is consistent with *State Planning Policy 2.4 - Basic Raw Materials* which identifies the site as a Key Extraction Area for retention and protection in the long term. The historic development of the site and subsequent planning determinations have formalised the nature of land use on this portion of the escarpment.

The operation of the asphalt plant will not result in any additional impacts upon clearing, excavation or public access that would affect the vegetation or landscape features of the escarpment beyond that which currently exist. On this basis the historic P&R designation does not represent an impediment to approval of the application.

Orderly and proper planning of the locality

Comments received during the submission period raise concerns regarding land use compatibility and associated impacts upon development and planning within the Orange Grove locality.

Properties in the vicinity of the quarry and along the main access routes to the site are generally characterised by small rural landholdings, within the General Rural zone of the City of Gosnells Town Planning Scheme No.6.

The draft City of Gosnells Foothills Rural Strategy (2004) was prepared by the City to provide guidance on future strategic planning within the locality. Although the Strategy has not been formally adopted and will likely require further review, it recognises constraints associated with the long term quarry operations taking place within the Boral facility and the need to limit intensification of sensitive land uses within 1 kilometre of the quarry, consistent with guidance contained within *State Planning Policy 2.4 Basic Raw Materials* (SPP 2.4).

From an operational perspective the establishment of an asphalt plant in association with the hard rock quarry will allow for efficiencies in raw material extraction and processing and would be consistent with the objectives of SPP 2.4.

The proposal will not place any additional impediment on subdivision or development within the locality beyond that which currently exists as a result of the quarry operations. The temporary nature of the development will also ensure that future strategic planning options for the locality will not be compromised.

Having regard to these factors, approval of the temporary MAP would not be detrimental to the orderly and proper planning of the locality.

Preservation of the amenities of the locality

The main concerns raised through submissions relate to the potential for adverse impacts upon the amenity of nearby landholdings, including properties located along the main truck access route.

The locality is broadly characterised by low density, semi-rural properties, set against the backdrop of the Darling Escarpment with its associated parks/ reserves, and an operational hard rock quarry. The quarry encompasses activities such as drilling, blasting, crushing/screening, storage and haulage of hard rock materials. In recognition of potential offsite impacts the quarry is subject to a generic 1 kilometre buffer under *State Planning Policy No. 2.4 Basic Raw Materials* to limit intensification of nearby sensitive land uses.

The proposed MAP operations have the potential to impact the amenity of the locality through emissions such as dust, noise and odour, as well as traffic and visual disturbance.

Emissions

The *Environmental Protection Authority Guidance Note 3 - Separation Distances between Industrial and Sensitive Land Uses* identifies a generic separation distance of 1 kilometre for Asphalt works having regard to potential offsite impacts of noise, dust and odour. This distance is intended as a default rather than absolute separation distance and may be reduced where appropriate scientific studies demonstrate that a lesser distance will not result in unacceptable impacts.

The three closest dwellings are located approximately 753m², 823m² and 892m² from the proposed MAP plant, with a further 7 lots wholly or partially within a 1 kilometre radius.

Information provided by Boral indicates that potential airborne emissions from the manufacturing process may include:

- Odour;
- Total suspended particulates
- Particulate matter less than 10 µm (PM₁₀) and 2.5 µm (PM_{2.5})
- Volatile organic compounds
- Asphalt and bitumen fumes
- Oxides of nitrogen (NO_x)
- Carbon monoxide; and
- Polycyclic aromatic hydrocarbons (PAH_s)

The primary source of emissions arising from the MAP is most likely to occur from the dryer stack, used to emit air from the rotating drum dryer, and through the process of filling delivery trucks.

Modelling data provided by Boral predicts emissions based upon data previously collected from the MAP and its operational Welshpool plant, as well as assumptions made regarding the proposed MAP location. The modelling indicates that siting of the MAP within the base of the Stage 2 quarry and modification of the stack (reduced diameter/ increased height of 15.5 metres) will assist in dispersal of the emissions through wind flow distortion and induced turbulence. The predictions conclude that all pollutant concentrations would be well below the respective standards and guidelines and that the MAP will be adequately separated from nearby sensitive receptors.

It is noted that a secondary source of odour may also be experienced through the transport of asphalt off site. To limit such impacts trucks are to be covered with tarpaulin, many of which will utilise an automatic tarping systems to further reduce odour emissions. Although Boral acknowledges that some temporary odour from passing trucks may arise, it is deemed that this will dissipate quickly and be an insignificant health risk. In order to limit such impacts, it is appropriate that restrictions be placed on the hours of operation and transport times to prohibit transport at night and on Sundays.

The Boral commissioned review of separation distance requirements for the proposed development, including noise modelling, also concludes that the MAP will be adequately separated from nearby sensitive receptors to manage the potential impacts of noise and dust. Existing noise and dust management measures currently in place on site will be adapted and/ or amended to incorporate the MAP process.

Environmental agencies have raised no critical concerns with respect to the acceptability of the proposal. The modelling methodology and conclusions provided by Boral will need to be verified by DER as part of its works approval and licencing process. This will include a commissioning phase where emissions data is collected to ensure that estimates are representative of the actual measurements taken.

Traffic

Access to the MAP is proposed to be obtained via the main Boral Quarry entrance off Stephen Street, via an existing quarry access road across Lots 50 and 181.

The main access route to the MAP will be via Reservoir Road, Maddington Road, White Road and Kelvin Road, which provides the most direct link to Tonkin Highway and the Gateway Project site, travelling in a northerly direction. This route is currently used by trucks servicing the established quarry operations.

Information submitted by Boral estimates current truck movements at 139 per operational day on average. Each 'truck movement' is calculated on the basis of a two way return trip. It is anticipated that an additional 3 daily truck movements (or six one way trips) and ten or less additional daily light vehicle movements would result from the proposal. This takes into account a reduction in quarry product exiting the site as well as the additional importation of materials required to service the MAP.

The overall traffic impact is considered minimal in the context of the established quarry operations. The City has also undertaken a preliminary analysis of the surrounding road network which also concludes that it meets expectations for the classification of the roads and is acceptable for operation of the MAP.

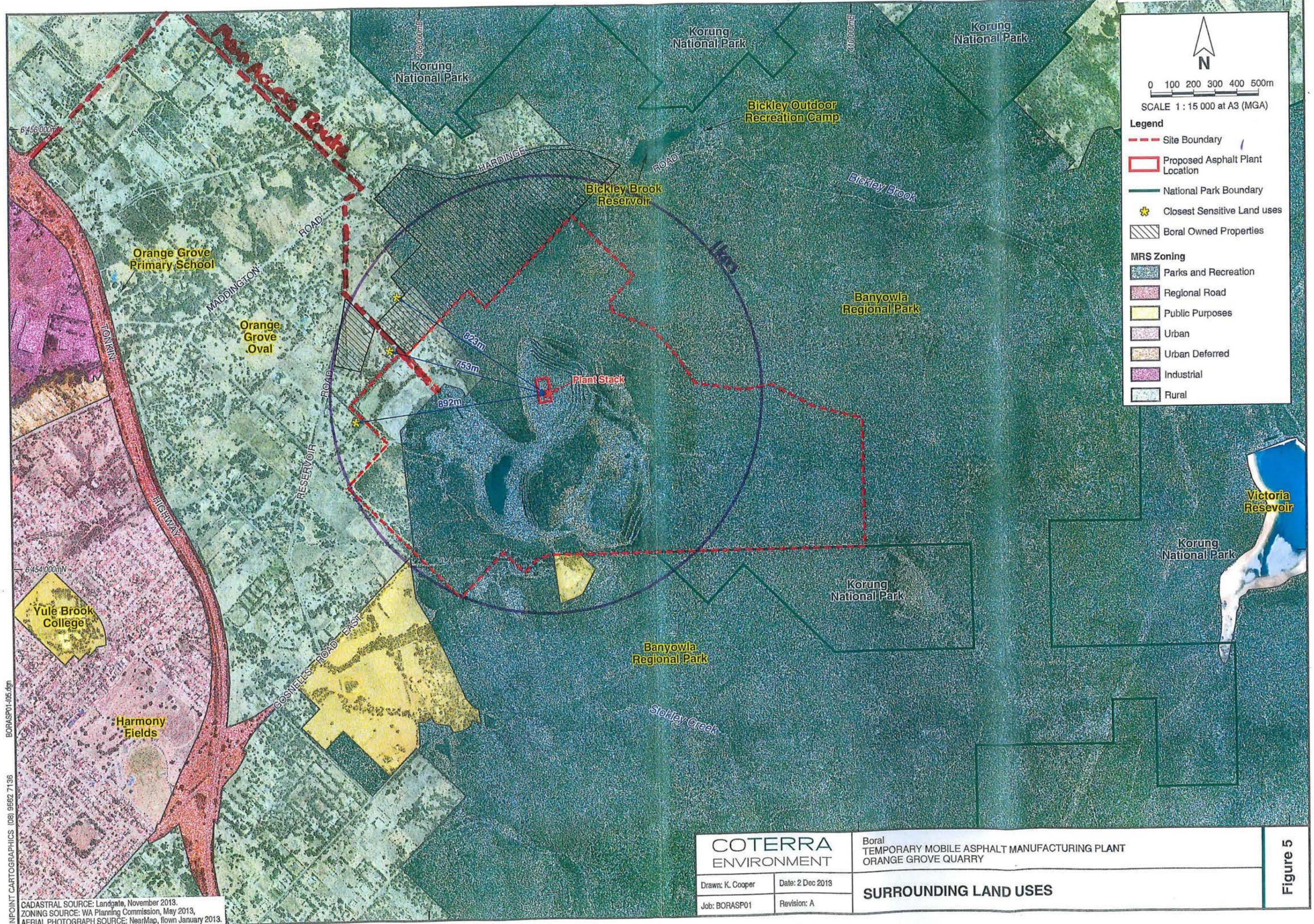
Visual Impact

The MAP will not result in any additional clearing or disturbance on site beyond that which already exists. Its location within the Stage 2 pit will also ensure that it is not visible from nearby rural areas or on the escarpment.

In the context of the established quarry operations and supporting information provided by Boral, offsite impacts likely to arise from operation of the temporary MAP would not have a significant detrimental impact upon the amenity of the locality.

Conclusion:

It is recommended that the proposal be approved subject to conditions relating to limitation on the period of approval and restrictions on the permitted times of operation.



PINPOINT CARTOGRAPHICS (08) 9662 7136
 BORASP01-005.dgm
 CADASTRAL SOURCE: Landgate, November 2013.
 ZONING SOURCE: WA Planning Commission, May 2013,
 AERIAL PHOTOGRAPH SOURCE: NearMap, flown January 2013.

COTERRA
ENVIRONMENT

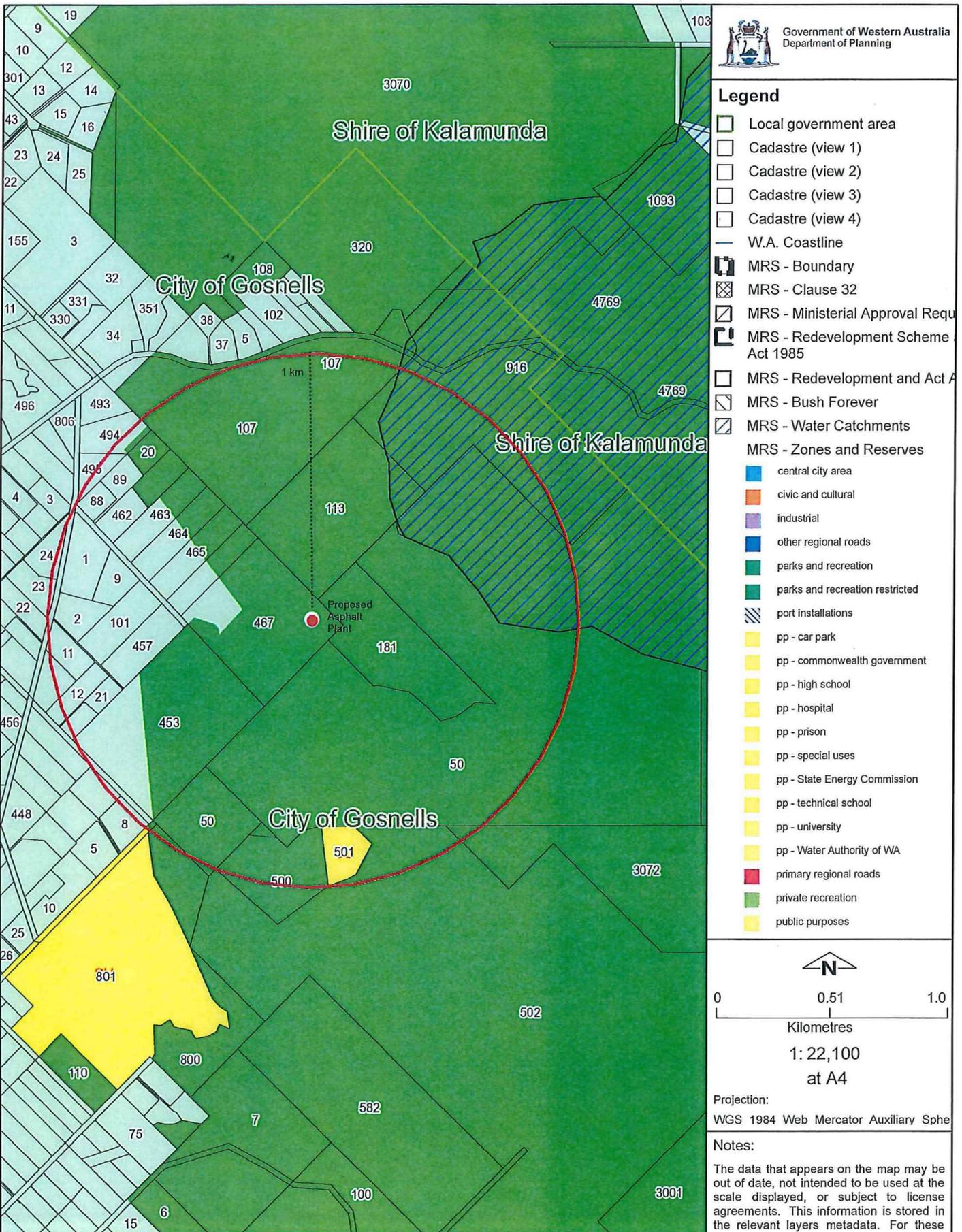
Boral
TEMPORARY MOBILE ASPHALT MANUFACTURING PLANT
ORANGE GROVE QUARRY

Drawn: K. Cooper Date: 2 Dec 2013
 Job: BORASP01 Revision: A

SURROUNDING LAND USES

Figure 5

ATTACHMENT 2



MRS Zoning

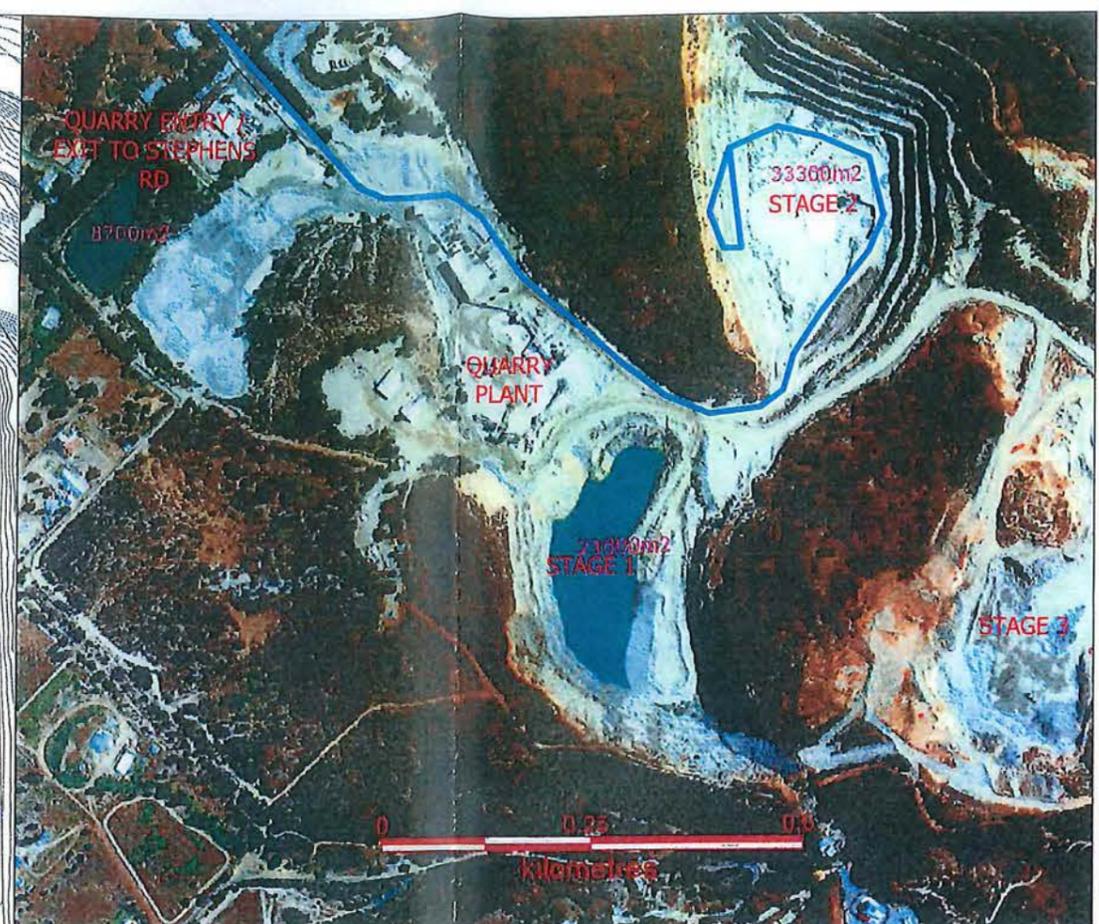
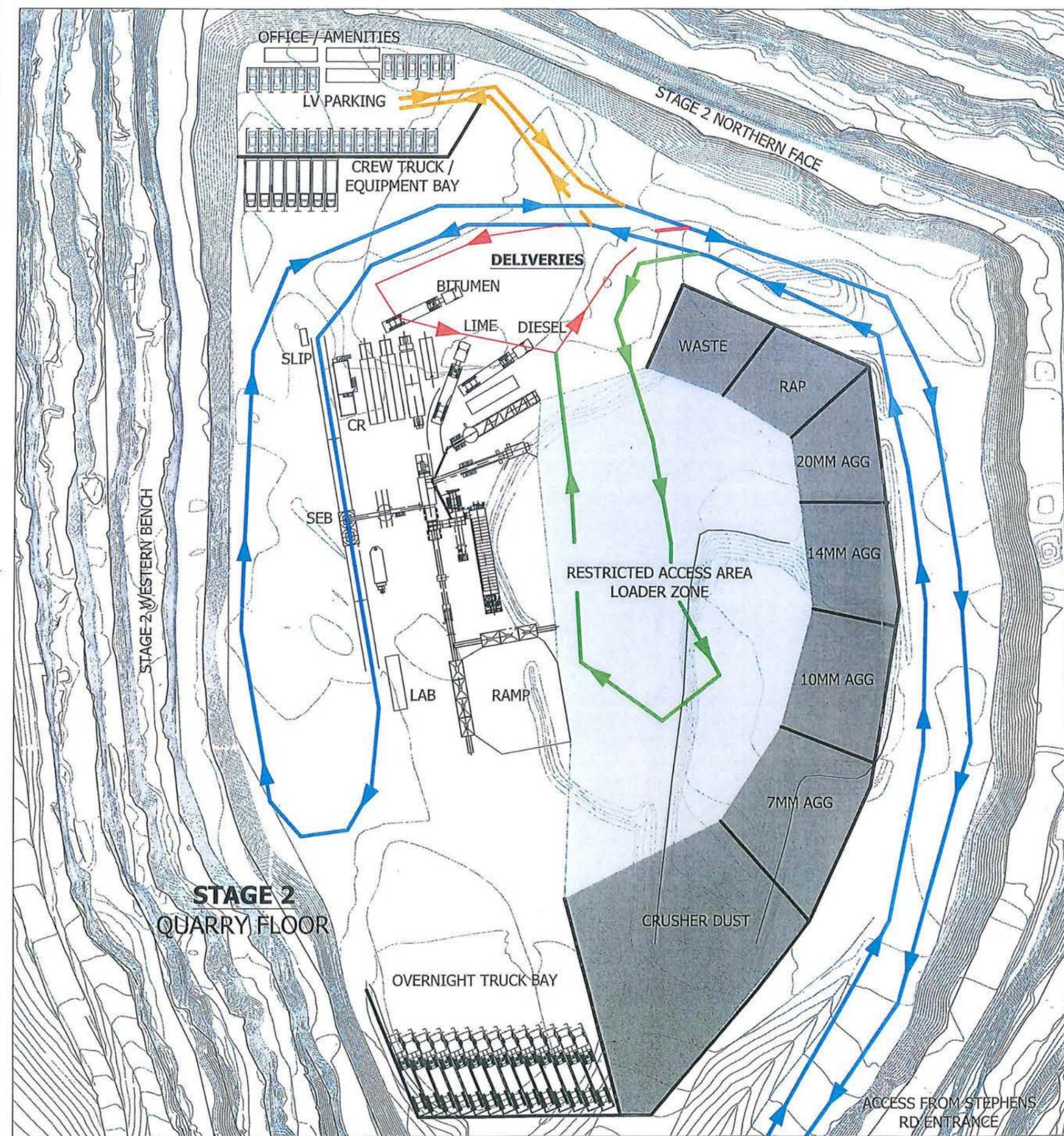
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Internal Spatial Viewer
[Link to viewer](#)

Produced by: FUNCTIONALITY TO COME

Date produced: 19-Mar-2014

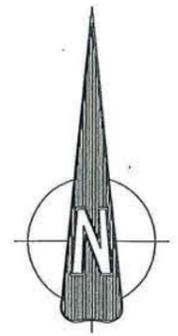
Notes:
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 Map was produced using DoP's PlanViewWA.



AV ORANGE GROVE QUARRY
Scale: NTS

- Legend**
- ASPHALT LOADING
 - AGGREGATE DELIVERIES
 - PRODUCT DELIVERIES
 - LIGHT VEHICLE ROUTE

1 PROPOSED SITE LAYOUT
Scale: 1:1000 @ A3



BORAL
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PHONE: +61 7 3268 8011 FAX: +61 7 3268 1071 ABN: 46 009 671 809

SITE ADDRESS:
WHINSTANES OFFICE
119 CULLEN AVENUE WEST
WHINSTANES,
QLD, 4007

POSTAL ADDRESS:
P.O. Box 227
HAMILTON CENTRAL,
QLD, 4007

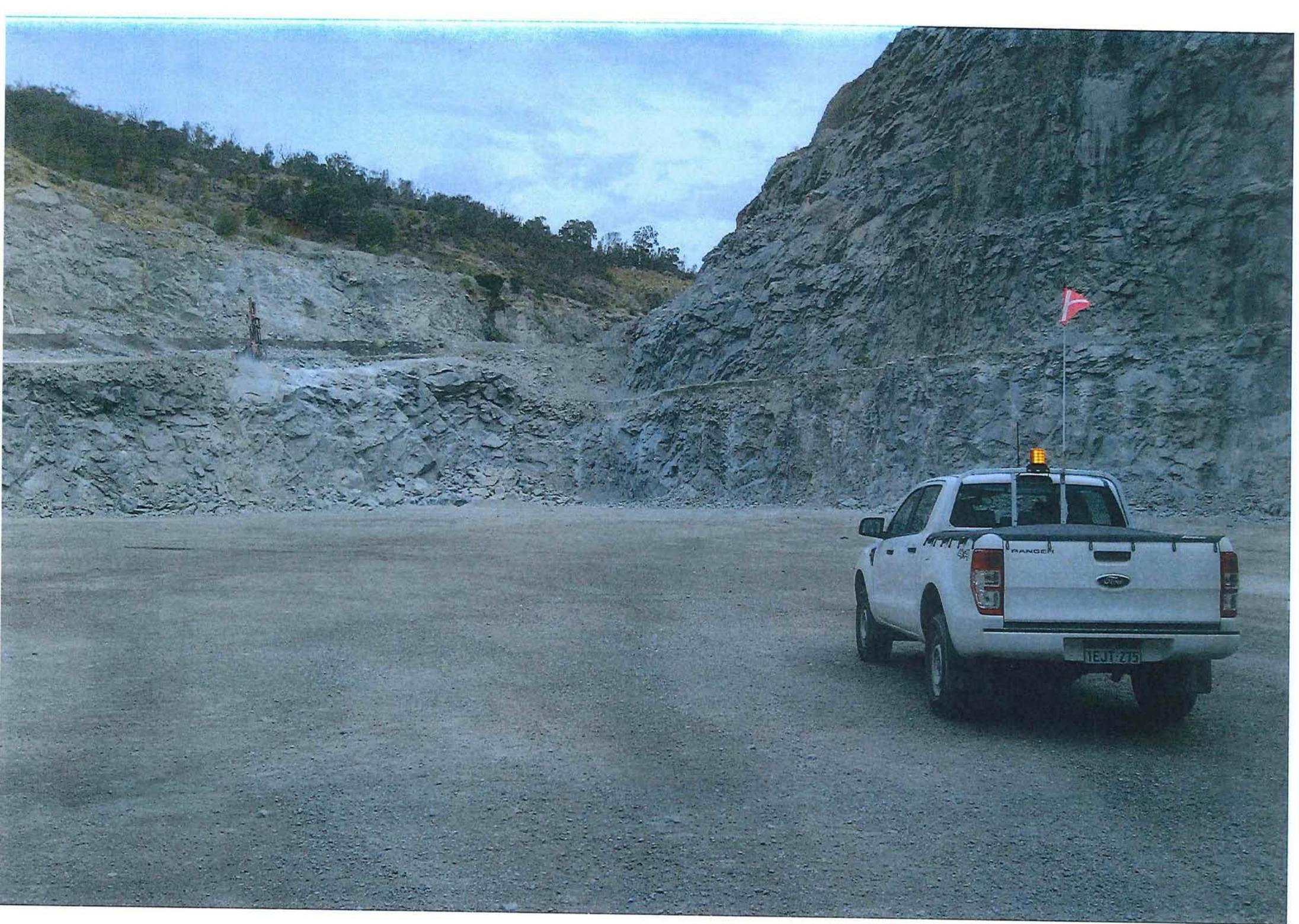
LEVEL DATUM: AHD
SCALE: 1:1000
DRAWN: -
APPROVED: -
ISSUE: DRAFT B

COORDINATE SYSTEM: A3
SHEET SIZE: A3
DATE: 19.11.13
DATE: -
DATE: -

CLIENT: BORAL RESOURCES (WA) PTY LTD
PROJECT: ORANGE GROVE QUARRY
STEPHEN STREET, ORANGE GROVE, WA, 6109
TITLE: PROPOSED SITE LAYOUT
T400 'ASTEC' MOBILE PLANT

JOB NUMBER: 0001	
DRAWING NUMBER: BCM_OG_MSD_001	
REVISION	REVISION DATE
A B C - -	19.11.13





Item 13.5.2 Continued

The quarry site currently has two Prescribed Premises operational licences, being:

- Category 12 operations (which includes screening of materials: premises on which materials are extracted from the ground is screened, washed, crushed, ground, milled, sized or separated), with an approved capacity of 2,000,000 tonnes per year.
- Category 13 operations (which includes crushing of building material: premises on which waste building or demolition material is crushed or cleaned), with an approved capacity of 40,000 tonnes per year.

In terms of the Category 12 operations, whilst the use is licenced to screen 2,000,000 tonnes per year, the applicant has advised that the quarry is currently producing approximately 1,000,000 tonnes per year.

Furthermore, in 2006 the City issued an Extractive Industry Licence in accordance with the Extractive Industry Licence Local Law 2000. This permits the continuance of the use until 30 June 2025.

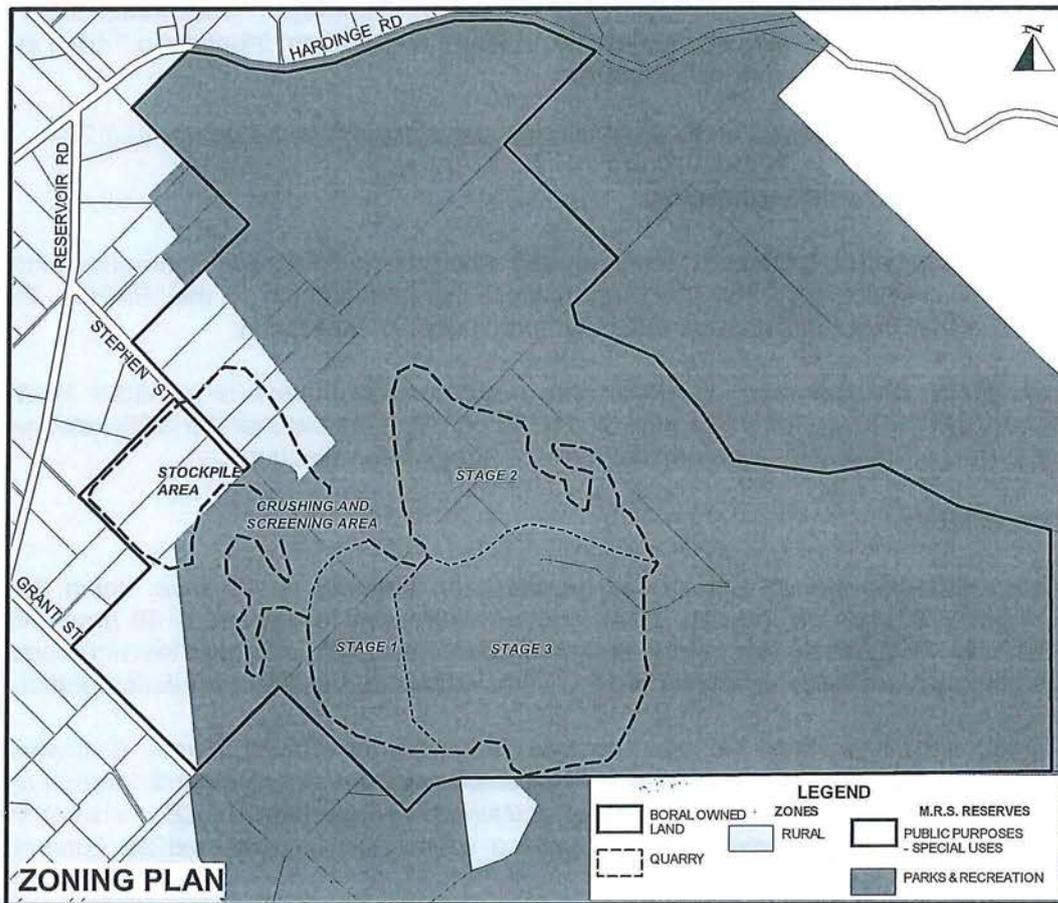
Site Description and Planning Framework

The quarry is located on the Darling Escarpment in Orange Grove, with the operation currently occupying approximately 73ha of the 271.12ha of land which the applicant owns, or leases. Land to the north, south and east of the quarry forms part of the Banyowla Regional Park. Land to the west of the site comprises a range of rural uses, such as rural-residential, hobby farms, orchards and a horse and pony club.

A majority of the quarry site is reserved for Parks and Recreation under the MRS. A smaller portion of the site, located around Stephen Street, is zoned Rural under the MRS, and General Rural under Town Planning Scheme No. 6 (TPS 6). The existing quarry pits and crushing/screening areas are located within the Parks and Recreation reserve and the entrance area, stockpile area, weighbridge and ponds are located within the portion of the site that has a Rural zoning.

A map identifying the location and the zoning of the subject site follows.

Item 13.5.2 Continued



Proposal

Boral has advised that the application involves the following aspects:

- Locating a mobile asphalt processing plant in the base of the Stage 2 quarry pit. This area is 97m below natural ground level and comprises 3.2ha. This area is contained wholly within the Parks and Recreation reservation.
- The plant would provide a maximum of 475,000 tonnes of asphalt to service the Gateway WA Perth Airport and Freight Access Project (ie upgrade of Tonkin Highway and Leach Highway near Perth Airport), being undertaken by the State Government.
- It is anticipated that the project will be completed towards the end 2016, after which time the asphalt plant will be removed from the subject site.
- The asphalt mixture will comprise of aggregates/dust/sand which are sourced from the quarry, therefore the raw materials that ordinarily would be transported for off-site processing will be able to be used on-site for the asphalt processing plant.
- It is anticipated that there would be an average of three additional truck movements per day and ten or less additional light vehicle movements during the site's peak hours on any typical day.

Item 13.5.2 Continued

- The typical operational hours for the plant will be between 5:00am and 5:00pm, Monday to Friday and 5:00am and 4:00pm on Saturdays. Truck movements in and out of the site will be between 6:00am and 6:00pm, Monday to Friday and 6:00am and 4:00pm on Saturdays.

The site plans and details on the asphalt plant are contained as Appendix 13.5.2A.

Planning Approval Requirement

The development is proposed wholly on land reserved as Parks and Recreation under the MRS. Accordingly, the City may make a recommendation to the WAPC. The WAPC is the Planning Approval Authority for this type of application.

Importantly, the evaluation of health and environmental impacts is a matter for the Department of Environment Regulation (DER). It is understood that the DER has been consulted by the WAPC to inform the WAPC's future determination.

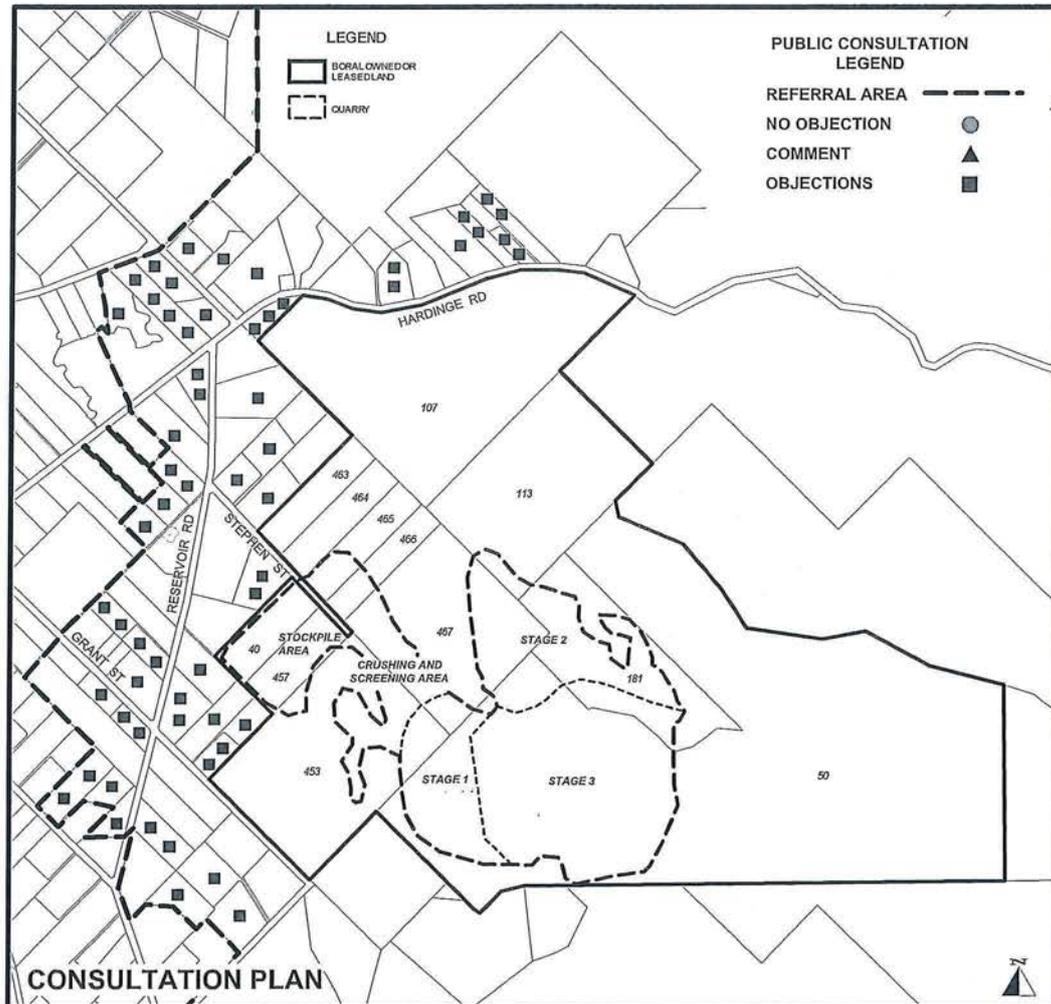
Consultation

Public comment was invited on the proposal for a period of 21 days, being from 3 January 2014 to 24 January 2014. Letters were sent to owners of 66 properties within the 300m consultation area, as well as seven occupiers of properties in the area. The proposal was also advertised on the City's website during the consultation period.

In response, 99 submissions were received during the advertising period, all of which objected to the proposal. A summary of these submissions and comments thereon are provided in the Schedule of Submissions contained as Appendix 13.5.2B. A list of the properties where the owner/occupier made a submission is contained as Appendix 13.5.2C.

A map identifying the consultation area and the origin of each submission follows. It should be noted that 39 of the submissions that were received are from submitters who are located outside of the consultation area.

Item 13.5.2 Continued



The main issues raised in the submissions are as follows:

- The appropriateness of the land use in the area and its compatibility with the site's reservation status.
- Environmental and health issues from toxic emissions, odour, and the impact on air quality.
- Dust.
- Additional heavy vehicle traffic, and the ability of the existing road system to accommodate heavy vehicle traffic and provide for a pedestrian environment that is adequately safe.
- Impacts on the rural amenity of the area.
- Noise as a result of the proposed land use.
- Hours of operation.
- The potential permanency of the use.

Item 13.5.2 Continued

Each is discussed in the following sections, along with any other applicable technical matters.

Separately, a public meeting was also held to discuss the proposal on 19 February 2014. At the meeting, an additional concern was raised which related to the existing commercial arrangement between Boral and the City. Boral leases land from the City in a long-term agreement. Although this is not a planning consideration, it is an issue that was raised by some of the people who attended the meeting.

DISCUSSION**Zoning and Land Use**

The subject site is reserved under the MRS for the purpose of Parks and Recreation, and abuts land that is zoned General Rural under TPS 6. A number of submissions raised concerns that the proposed use is industrial in nature and incompatible with the Parks and Recreation reservation or the adjoining General Rural zoning.

Whilst it is considered that the current and proposed use of the site does not conform to the purpose of the reservation, the quarry has been approved in this location since the 1960s, and is expected to remain in operation for years to come given its importance in providing a major source of hard rock construction material for the Perth Metropolitan area.

State Planning Policy 2.4 - Basic Raw Materials (SPP 2.4) highlights the need to protect existing extraction operations by preventing the intrusion of incompatible or sensitive uses such as rural-residential, that may be prejudicial to extraction activities, particularly in regard to noise, dust, odour and vibration complaints. As a result of SPP 2.4, the City's Draft Foothills Rural Strategy imposes a 1km buffer (radius) from the operational areas of the quarry, where the introduction of sensitive land uses (including via subdivision) is not to be supported by the City.

In considering the appropriateness of the asphalt plant land use in the proposed location, Council should have regard to the existing buffer of the quarry operation, the recommended buffer distance of the proposed asphalt plant, and the impacts of the uses on the surrounding area.

The Environmental Protection Authority's (EPA) Guidance Statement 3 stipulates guidelines for the separation distances between industrial and sensitive land uses (in accordance with the EP Act), based on the typical impacts that may be received by nearby sensitive land uses. The distances outlined in the Guidance Statement are not intended to be absolute separation distances, rather they are a default distance for the purpose of:

- Identifying the need for specific separation distances or buffer definition studies; and
- Providing general guidance on separation distances in the absence of site-specific technical studies.

Item 13.5.2 Continued

In terms of the asphalt plant use, the Guidance Statement recommends a buffer distance of 1km from sensitive land uses. The nearest sensitive land use is a rural-residential dwelling that is 753m from the proposed location of asphalt plant. In order to address this, the applicant has provided technical studies to justify how land use impacts will be managed to an acceptable level. This is discussed later in this report.

As mentioned previously, consideration of the appropriateness of the proposed land use ultimately lies with the WAPC. Notwithstanding this, given that the site already contains an existing, long-term, industrial land use, where land use buffers have been established to favour the continuity of the use, it is not considered unreasonable for a related land use to be established on the site providing that emissions and traffic movements can be adequately managed.

Environmental Factors

Although the City is not the expert authority and is not being asked to make a recommendation on this element of the application, the applicant has provided a detailed submission to address these aspects of the proposal.

The following sections summarise these aspects for the information of the Council.

The proposed use has the potential to result in dust and odour emissions. To address these matters, the applicant has provided relevant technical studies for EPA assessment to demonstrate that anticipated impacts from the use can be appropriately managed. The assessment is summarised below and overleaf, for the information of Council.

Firstly, it should be noted that the applicant sought advice from the EPA prior to lodging the subject application as to whether the proposal would warrant a formal referral to the EPA. The quarry operation was given approval under Part IV of the EP Act by the Minister for Environment in 1991. The EPA has advised that the mobile asphalt plant can be adequately managed under the existing conditions and commitments set out in its 1991 approval, as well as through an amendment to the quarry operational licence under the EP Act.

Air Emissions

A majority of the submissions received raised concerns about airborne emissions from the plant, including odour and toxic emissions, and the impact that these would have on the air quality, the health of residents, the environment and local water supplies.

The applicant has advised that the potential airborne emissions from the asphalt production process include:

- Odour;
- Total suspended particulates (TSP);
- Particulate matter less than 10 µm (PM₁₀) and 2.5 µm (PM_{2.5});
- Volatile organic compounds (VOCs);
- Asphalt and bitumen fumes;
- Oxides of nitrogen (NO_x);

Item 13.5.2 Continued

- Carbon monoxide; and
- Polycyclic aromatic hydrocarbons (PAHs).

The applicant has advised that the potential sources of emissions from the mobile asphalt plant will be as follows:

- The dryer stack used to emit air from the rotating drum dryer which primarily contains dust, VOCs and combustion products from the burners. This air is passed through a filter system where over 99.9% of the particulates are removed.
- The process of filling the delivery trucks - This occurs by the trucks moving slowly under the storage silos where the asphalt mix is delivered from the bin chute into the truck tray over a typical time period of 5 to 10 minutes. The drivers then cover the hot asphalt with a tarpaulin prior to departure from the site.
- Bitumen kettles that store and heat the different qualities of bitumen binding agents. These contain breather pipes which are fitted with water stops at the discharge end to control any fumes displaced during the bitumen unloading process.
- Hot mix silos and the conveyor to the silos. The conveyor is fully enclosed so that emissions are contained within the headspace of the silos.

The applicant advised that the potential for emissions from the latter two processes mentioned above is negligible.

The applicant engaged Air Assessments (consultants) to undertake an assessment of the likely emissions from the mobile asphalt plant. The following conclusions were drawn by the consultants:

- The asphalt plant is proposed to be located within the Stage 2 pit, being 97m below natural ground level, resulting in steep walls around the pit, with only a small entrance to the south. Given the terrain, it is expected that the airflow will be distorted such that any plume reaching nearby receptors (closest residence is 753m to the west) will have been dispersed to a greater extent than what would occur for simple, flat terrain.
- Odour levels are expected to be well within the limits adopted by the DER for the assessment of the application.
- Pollutant concentrations will be below respective standards and guidelines that are accepted in Western Australia.
- There might be a temporary odour impact from passing trucks, however this is expected to be limited, dissipate quickly, and would pose an insignificant health risk.

As mentioned previously, the applicant will be required to submit a works approval licence application to the DER, where the proposal will be assessed to ensure that it is environmentally acceptable and manageable.

*Item 13.5.2 Continued*Dust

The applicant has advised that the main sources of dust from the mobile asphalt plant are:

- The dryer stack used to emit air from the rotating drum dryer;
- Dust from vehicles; and
- The movement and storage of materials.

The applicant has proposed a filter system to regulate dust caused by the process, which will be evaluated by the DER.

In terms of dust generation from vehicle movements and storage, the following measures are proposed:

- All trucks transporting raw material aggregates will have covered loads;
- All vehicles leaving the site will go through a washdown facility;
- Sprinklers will be in operation in the raw material storage areas; and,
- Filler silos will be fitted with fabric filter dust collectors and a high-level switch with an alarm. The switch controls a shut-off valve on the filling line, which automatically closes once the filter reaches a high level.

As mentioned previously, the environmental aspects of the proposal will be considered by relevant state government bodies and determined by the WAPC. The proposal is also understood to require a works approval permit from the DER.

Traffic

A number of concerns were raised by submitters about the existing, heavy vehicle traffic, expected increases in traffic as a result of this proposal, and the adequacy of the surrounding road network and infrastructure to safely accommodate heavy vehicle traffic. These matters are discussed below.

Vehicle Movements

The asphalt mixture will comprise raw materials sourced from the quarry. Therefore raw materials that ordinarily would be transported off site, will be used for the asphalt processing plant. After taking into account the materials which will need to be brought on site to produce asphalt, the applicant anticipates that there would be an average of three additional, two-way truck movements per day, and ten or less additional light vehicle movements per day during the site's peak hours on any typical day.

Existing and predicted truck movements on the site have been calculated by applying the following criteria:

- The quarry currently excavates 1,000,000 tonnes of hard rock, however it is licenced to excavate 2,000,000 tonnes.

Item 13.5.2 Continued

- The Gateway WA Perth Airport and Freight Access Project requires 500,000 tonnes of asphalt, with 475,000 tonnes of that proposed to be produced from the quarry site, and 25,000 tonnes will be sourced from an asphalt plant in Welshpool.
- The project is expected to be completed in late 2016, and therefore the asphalt plant may remain on the site for 2.5 to 3 years, after which time it will be removed from the site.
- Asphalt mixture comprises approximately 94% raw materials (sourced from the quarry), 4.5% bitumen and 1.5% lime. As such, out of the 475,000 tonnes of asphalt required, 446,500 tonnes will be site sourced raw materials, 21,375 tonnes will comprise of bitumen and 7,125 tonnes will comprise of lime. In addition, 2,850,000 litres of burner fuel will be required to operate the plant.
- In terms of truck load capacity, raw materials and asphalt are carried in 30t loads; bitumen will be carried in 45t loads; lime will be carried in 25t loads; and burner fuel will be carried in 30,000 litre loads.
- There are 312 working days per annum.

The below table outlines the calculation of the current average two-way vehicle movements associated with the quarry operation and the amount of movements that could occur under the current licence arrangements.

Amount of Aggregate Per Year	Truck Movements Per Year (based on 30t loads)	Average Truck Movements Per Day (based on 312 working days per year)
1,000,000 (current)	33,333	107
2,000,000 (future)	66,666	214

Based on the information provided, the following tables outline the calculation of the expected, daily, overall, average vehicle movements for the quarry and mobile asphalt plant. It should be noted that the applicant anticipates that the actual amount of asphalt required for the Gateway project would be more like 450,000 tonnes, and as such the figure of 475,000 tonnes represents a worst case scenario.

Item 13.5.2 Continued

Item	Approximate Load over Project Life (2.5 years)	Two-Way Truck Movements for Project Life (2.5 years)	Two-Way Truck Movements Per Year (312 working days)	Two-Way Truck Movements Per Day
Quarry Operation				
Raw material based on 821,400 tonnes per annum * figure above is based on total aggregate quarried per annum (1,000,000) minus total aggregate used for asphalt production that is not transported off site per annum (178,600).	2,053,500	68,450	27,380	88
Asphalt Production				
Incoming				
Bitumen 45t loads	21,375	475	190	0.61
Lime 25t loads	7,125	285	114	0.36
Burner Fuel 30,000 litre loads	2,850,000	95	38	0.12
Outgoing				
Finished Asphalt	475,000	15,833	6,333	20.29
Total		85,138	34,055	110

As shown on the above table, based on current production levels, it is anticipated that there would be an average increase of three, two-way truck movements (or six one-way truck movements) as a result of the proposed asphalt plant. It is considered that this increase and the proposed ten or less additional two-way light vehicle movements are not significant, and are broadly consistent with current operational levels. This is particularly the case when it is considered that 214 truck movements per day could occur under current licensing arrangements.

Road Infrastructure

Whilst the proposal does not result in a significant increase in traffic volumes, the surrounding road network (including Stephen Street, Reservoir Road, Maddington Road, White Road, Gosnells Road East and Kelvin Road) has been assessed to determine whether it is performing at acceptable levels. In this regard, the following should be noted:

- Several of the roads are rural in nature, unkerbed and some with open drains, while others in more built up locations have been widened, kerbed and drained.
- Intersections are mainly kerbed and line-marked and have been widened to handle the larger turning paths of the truck transport used by the quarry.
- Main Roads WA crash data for the five year period between 1 January 2008 and 31 December 2012 has been accessed for the intersections of Kelvin Road/White Road, White Road/Maddington Road, Maddington Road/Reservoir Road, Reservoir Road/Stephen Street, Reservoir Road/Grant Street, Reservoir Road/Staniland Street and Reservoir Road/Gosnells Road East.

Item 13.5.2 Continued

Two crashes were recorded at the intersection of Reservoir Road and Grant Street during the five year period, however these crashes both occurred outside quarry operating times.

- Traffic volumes and speeds for the local roads were assessed and it was found that the 85th percentile speeds were generally well in excess of the posted 60km/h speed limits, with the exception of Stephen Street. This could be attributed to the rural nature of the area, generally low traffic volumes, the nature of through traffic and a lack of speed enforcement in the area.
- Other than Stephen Street and Grant Street, the local roads are classified as Local Distributor Roads under the City's road hierarchy and are performing within expectations for this classification.
- Main Roads WA has designated all of the abovementioned roads (excluding Grant Street and Staniland Street) as restricted access vehicle routes, which means that these routes are suitable for heavy vehicle traffic for which permits are required.

Based on the above, namely the relatively low traffic volumes, lack of crash history and the likely minimal increases in heavy vehicle movements as a result of the proposed mobile asphalt plant, the surrounding road network meets expectations for the classification of the roads and is considered to be acceptable. Whilst traffic speeds are of concern for these roads, this is a separate matter for which responsibility rests with Police.

In terms of comments raised by submitters about the need for footpaths and/or bridle trails so that it is safe for pedestrians and horse riders to traverse the local roads, the following comments are provided:

- The area is low-density and characterised by large lots by virtue of being zoned for rural purposes.
- The majority of the roads in the area have open drains on both sides. Consequently, footpath installation would mean that the drains would have to be piped which would be costly and difficult.
- The area is not a high priority on the City's footpath priority matrix due to its location away from major pedestrian generating uses (ie commercial areas, schools, etc).

For the above reasons, there are no future plans to construct footpaths or bridle trails in the area surrounding the quarry.

Amenity

A number of submissions were received during the public consultation period which raised concerns regarding the potential for the development to impact on the amenity of the area by virtue of the hours of operation, adverse noise (including noise from heavy vehicle traffic) and impacts on the visual amenity of the area.

*Item 13.5.2 Continued*Hours of Operation

The applicant has advised that the typical operational hours for the plant will be from 5:00am to 5:00pm, Monday to Friday and 5:00am to 4:00pm on Saturdays. Truck movements in and out of the site will be from 6:00am to 6:00pm, Monday to Friday and 6:00am to 4:00pm on Saturdays. Furthermore, works are not proposed for Sundays and during the evenings, and if product is required outside of operational hours, then asphalt product will be sourced from an existing asphalt plant facility in Welshpool.

Notwithstanding the above, the applicant has suggested that in order to meet peak supply demands or in the event of emergency situations (such as if asphalt cannot be sourced from the Welshpool asphalt plant due to a mechanical failure), the mobile asphalt plant may need to occasionally operate outside of the above stated hours.

In considering this, whilst the typical operational hours are considered to be acceptable, it is concerning that adverse noise or heavy truck movements could occur by virtue of the use operating on Sundays and in the evenings, even on an occasional basis. Therefore, the timing of truck movements to and from the site would need to be limited to the anticipated hours as stated by the applicant. This would restrict heavy vehicle movements on the surrounding road network from occurring outside of reasonable hours.

Noise

A number of submissions were received during the advertising period which raised concerns about the current noise levels, and the potential increase in noise levels as a result of the proposal. In response to this, the applicant has identified that the proposal has the potential to produce noise by way of:

- During the construction phase noise may be generated from site preparation, including ground works, construction machinery/tools and construction vehicles.
- The mobile asphalt plant itself: drying drum gas burner, baghouse fan motor, baghouse exhaust, air compressor, raw material conveyor and vibratory hopper.
- Haul trucks and front end loaders.

It should be noted that noise emissions in Western Australia are regulated through the Environmental Protection (Noise) Regulations 1997. The Regulations assign maximum noise levels that are allowed to be received at a noise-sensitive premise at a particular time of day.

The applicant engaged Lancall Occupational and Environmental Consultants to assess the potential noise impacts from the mobile asphalt plant on nearby sensitive receptors.

The consultants noted that base line results achieved from noise monitors indicated that in some cases the noise generation exceeded the noise criteria of the Environmental Protection Regulations due to the passing of trucks near the noise monitors. It should be noted however that the regulations do not apply to traffic or vehicle noise.

Notwithstanding the existing use, the noise assessment of the mobile asphalt plant provides the following conclusions:

Item 13.5.2 Continued

- The predicted noise levels at the nearest noise sensitive receivers from the proposed asphalt plant are compliant with the assigned noise levels within the Regulations.
- The predicted noise levels from the proposed asphalt plant will have no discernible effect on the existing noise levels at the nearby receivers (adjoining properties).
- The minimal increase of traffic visiting the site as a result of the proposed mobile asphalt plant will have limited effect on the existing traffic noise levels.

Visual Amenity

Concerns were raised by submitters regarding current and future visual amenity impacts in the form of the 'Darling Scar' and views of the proposed mobile asphalt plant, including the emission stacks required for the operation.

As mentioned previously, the asphalt plant is proposed to be located in the existing Stage 2 pit, which is approximately 97m below natural ground level. The applicants lease or own a significant area around the Stage 2 pit. There will be no views of the operation in the pit from surrounding publicly accessible land.

Whilst the mobile asphalt plant will have a 15.5m high stack, used for the purpose of minimising dust emissions, given that it is located within a pit that is 97m below natural ground level, it will not be visible to the public, nor would any other components of the mobile asphalt plant operations.

Rural Amenity

In considering whether a proposal will impact on the amenity of an area, it is important to consider the nature/characteristics of the existing rural amenity and what changes the proposal is likely to have on that amenity.

The area surrounding the quarry to the west is zoned for rural purposes, and contains mainly rural-residential uses on larger blocks of around 1-2ha. The amenity of the area is affected by the existing quarry, by virtue of noise, traffic, dust and vibration. As previously mentioned, this is recognised by the draft Foothills Rural Strategy as a reason to restrict the intensification of sensitive uses within this area.

In terms of the Parks and Recreation reserved land to the north, south and east of the quarry, this area is a designated Regional Park and it is considered that the amenity for this area will not be impacted further by this proposal.

In terms of the proposed application, an asphalt plant has the potential to result in odour and emissions, however this would be subject to licencing and control by the DER and a decision on this element of the proposal will be made by the WAPC. Otherwise, the proposal is not anticipated to result in any significant traffic, noise or visual impacts beyond what is already in existence. Furthermore, the proposed asphalt plant is intended to operate for a limited period of up to three years. On this basis it is considered that the proposed use will not have any major impact on the existing amenity of the area.

*Item 13.5.2 Continued***Potential Conflict of Interest**

Informal feedback has raised concerns about a potential conflict of interest due to the City's separate and longstanding existing lease agreement with Boral.

From a planning perspective, the decision making process allows the Council to make a recommendation to the WAPC based only on planning considerations.

In order to provide an external perspective on this aspect of public feedback, the City has obtained legal advice which confirms that although the City has a financial interest in a portion of the quarry this does not prevent it from making a recommendation to the WAPC.

Term of Operation

The purpose of the mobile asphalt plant is to serve the Gateway WA Perth Airport and Freight Access Project, which is expected to be completed in late 2016. As such, the mobile asphalt plant is expected to remain on the site for 2.5 to 3 years, after which time it will be removed.

To ensure that the site does not become a permanent location for the mobile asphalt plant, it is suggested if the WAPC does resolve to approve the project, consent should be for a limited duration to support the Gateway Project.

CONCLUSION

The analysis of this proposal leads to a number of key findings, as follows:

- The area contains an existing long-term industrial land use and buffer area, all of which form a part of the diverse character of the area.
- The proposed location of the asphalt plant is at the bottom of the Stage 2 pit which is 97m below natural ground level and approximately 753m from the nearest residence.
- It is understood that the use will be subject to a works approval licence from the DER which will assess whether the use is environmentally acceptable and manageable.
- Concerns over impacts on environmental grounds and health impacts are matters for the WAPC to consider as this body will determine the application.
- The increase in vehicle movements from the proposal is estimated to be minimal and the existing road network has sufficient capacity to manage the anticipated increases.
- The predicted noise levels from the proposed mobile asphalt plant will comply with the Environmental Protection (Noise) Regulations 1997 and are anticipated to have limited effect on the existing noise levels at the nearby receivers.
- The use is not expected to impact greatly on the existing amenity of the area, given the relatively limited anticipated increase in vehicle movements.
- The use is only anticipated to be temporary in nature and any WAPC determination should reaffirm this aspect of the proposal.

Item 13.5.2 Continued

FINANCIAL IMPLICATIONS

Nil.

STATUTORY IMPLICATIONS

Determination of the Planning Application rests with the WAPC.

VOTING REQUIREMENTS

Simple Majority required.

STAFF RECOMMENDATION

That Council:

1. Forwards a copy of its report on the proposed asphalt plant within the Boral Quarry to the Western Australian Planning Commission to inform its decision-making process
2. Forwards a copy of all submission received from the public to the WAPC for its consideration in relation to the Boral proposal.

DEVELOPMENT APPLICATION - MOBILE ASPHALT PLANT
BORAL QUARRY, ORANGE GROVE

Schedule of Submissions

Response	Submission Reference Number (SR No.)	Total
Objection	All	99
Comment	-	0
No Objection	-	0

Matter Raised	Comment	SR No.
1. Zoning and Land Use		
The quarry operates on land zoned rural and reserved as Parks and Recreation. The land use proposed is industrial in nature and is not compatible with the zoning.	See comments under the heading Zoning and Use in the Discussion section of the report.	1, 2, 3, 4, 7, 8, 10, 13, 14, 15, 16, 17, 18, 19, 21, 23, 24, 25, 26, 30, 31, 32, 35, 36, 37, 38, 39, 43, 45, 49, 51, 53, 54, 58, 60, 65, 66, 69, 70, 71, 72, 73, 74, 75, 76, 78, 80, 83, 87, 88, 89, 90, 91, 92, 95.
Subdivision of properties within the quarry buffer zone have been blocked by the City of Gosnells and the Western Australian Planning Commission due to reasons that it would not be compatible with the rural life style of the area. This plant will not fit into a rural area and would further prevent subdivisions from occurring.	See comments under the heading Zoning and Use in the Discussion section of the report.	1, 2, 4, 10, 19, 43, 53, 54, 57.
The proposed development does not support the City's Foothills Rural Strategy.	See comments under the heading Zoning and Use in the Discussion section of the report.	1, 4, 10, 23, 35, 43, 53, 54, 69, 73, 91.
The Environmental Protection Authority's guidelines recommend that there be a 1km buffer from an asphalt plant to any sensitive land uses. The use would be near a number of highly sensitive land uses including a primary school, aged care facilities and an equestrian centre.	See comments under the heading Zoning and Use in the Discussion section of the report.	2, 5, 8, 16, 25, 38, 53, 54, 60, 90.
Suggests rezoning the surrounding land industrial or composite residential/light industry.	The City's draft Foothills Rural Strategy does not envision light industrial uses in this area, and as such, the City is not considering changing the zoning of the land surrounding the Boral site. The Strategy does however discourage the introduction of more intensive, sensitive land uses within the quarry buffer area.	1, 71.

Matter Raised	Comment	SR No.
<p>Boral is trying to establish a noxious industry in an area that is currently zoned General Rural. Before Boral commences the process to have this area rezoned, Council should be reminded of the public outcry that was the result of an attempt to rezone areas of Orange Grove previously.</p>	<p>The site is zoned Parks and Recreation. There is no application to rezone the land. This is a development application.</p>	<p>65.</p>
<p>2. Environment and Health</p>		
<p>Concerned about environmental and health issues from toxic emissions and impact on air quality.</p>	<p>See comments under the heading Air Emissions in the Discussion section of the report.</p>	<p>1, 2, 3, 4, 5, 8, 9, 10, 12, 15, 18, 21, 22, 23, 24, 25, 26, 27, 29, 30, 31, 32, 33, 35, 36, 37, 38, 39, 42, 43, 45, 47, 49, 52, 53, 54, 55, 57, 58, 60, 65, 66, 69, 71, 73, 74, 75, 77, 78, 79, 80, 81, 82, 85, 86, 87, 88, 90, 91, 92, 93, 94, 95, 96.</p>
<p>The quarry operation is currently producing dust which is impacting surrounding landowners.</p>	<p>Dust emissions from the existing quarry operations is governed by the works approval which is issued, administered and enforced by the Department of Environment Regulation. As such, it is considered that this is a separate matter that is not directly related to the subject application.</p>	<p>1, 2, 4, 7, 8, 10, 11, 19, 21, 24, 25, 26, 32, 33, 35, 36, 37, 38, 43, 46, 52, 53, 54, 55, 57, 59, 60, 66, 67, 71, 72, 73, 77, 78, 84, 86, 92, 93</p>
<p>Concerned that the proposed use will generate more dust.</p>	<p>See comments under the heading Dust in the discussion section of the report.</p>	<p>2, 4, 6, 10, 11, 12, 21, 24, 26, 32, 33, 36, 37, 38, 43, 46, 52, 55, 57, 59, 60, 65, 66, 67, 71, 77, 78, 86, 88, 89, 90, 91, 92, 95, 96.</p>
<p>Concerned that the use will produce odour.</p>	<p>See comments under the heading Air Emissions in the Discussion section of the report.</p>	<p>2, 3, 4, 7, 8, 10, 11, 12, 23, 31, 32, 34, 35, 36, 37, 38, 39, 42, 43, 45, 46, 49, 51, 53, 54, 58, 59, 66, 71, 73, 77, 80, 81, 89, 90, 91, 92, 93.</p>
<p>Concerned that the proposed use will impact on local water supplies of residents in the area and the local water catchments areas.</p>	<p>See comments under the headings Air Emissions and Dust in the Discussion section of the report.</p>	<p>2, 3, 4, 5, 8, 10, 16, 23, 33, 37, 43, 65, 69, 83, 91, 92, 96.</p>

Matter Raised	Comment	SR No.
Concerned about the impact the proposal will have on wildlife, livestock, crops and the environment.	In terms of concerns about the impact that emissions would have on the air quality and health of the area, see comments under the heading Environment in the Discussion section of the report. For the concerns raised about animals being hit by trucks, this is beyond the control of the applicant and the City.	5, 7, 16, 22, 26, 33, 34, 35, 41, 45, 49, 65, 83, 91, 92, 95, 96.
Boral Quarries cannot guarantee zero emissions from all these industrial activities as it is only 'anticipated' that they will comply - this is misleading.	The management and regulation of the use would be controlled by works approval issued by the Department of Environment Regulation. Should the operators breach their works approval, they may be subject to penalties from the Department of Environment Regulation.	1, 10, 37, 38, 53, 54, 73, 83
Concerned about how the site will be regulated/monitored to ensure that the use complies with all standards and guidelines, and whether any penalties will be imposed should non-compliance occur.	The management and regulation of the use would be controlled by works approval issued by the Department of Environment Regulation. Should the operators breach their works approval, they may be subject to penalties from the Department of Environment Regulation.	1, 4, 43, 49, 59, 73, 81, 83, 86, 88, 91.
Boral's environmental management should be viewed with suspicion given previous breaches in the past.	The management and regulation of the use would be controlled by works approval issued by the Department of Environment Regulation. Should the operators breach their works approval, they may be subject to penalties from the Department of Environment Regulation.	1, 2, 4, 8, 10, 25, 32, 37, 53, 54, 59, 66, 72, 73, 88, 91.
In Boral's Environmental Management Plan (EMP) submitted to the City and State Government agencies, Boral states that the three crushers are going to be shifted into Stage 2 to reduce noise and dust pollution once mining ceases there; however by placing the asphalt plant in the Stage 2 area, Boral will be reneging on the EMP commitments	Upon the decommissioning of the mobile asphalt plant from the Stage 2 quarry pit, the operators intend to recommence quarry activities in this location in accordance with their operational licence. As such, mining has yet to cease in this location.	1, 10, 53, 54
The City's Extractive Industries Bylaws list a rather low fine of \$400 for a pollution event, which is not a deterrent for a multinational \$1 billion corporation like Boral.	The Extractive Industries Local Law is not relevant in the case of this application.	1, 10, 53, 54
Patterson's Curse started on Boral's property and spread to the City's bridal trail and neighbouring properties requiring annual calls to spray the weed.	This is not relevant to the subject application.	1
Will Council support my request to the Government for an environmental impact assessment to be performed for this proposal.	See comments under the heading Environment in the Discussion section of the report.	16, 90

Matter Raised	Comment	SR No.
<p>The site is located in close proximity to large tracts of bushland that pose a high fire risk, should the asphalt plant catch fire from an ember attack there is a very real chance of an environmental disaster.</p>	<p>The applicant has advised that it will prepare a risk assessment specific to the subject proposal, including an emergency response plan covering off such risks as fire and storage and handling of combustible liquids.</p> <p>Such matters are likely to form part of the environmental and risk assessment associated with the works approvals issued by the Department of Environment Regulation.</p>	<p>32,</p>
<p>Concerned about vehicle accidents/spills involving chemicals, asphalt and bitumen on local road near properties, and also the potential for bitumen tankers to explode.</p>	<p>The applicant has advised that the storage and handling of all hazardous and dangerous goods will be in accordance with the relevant standards and a risk assessment and emergency response plan to be formulated for the use.</p> <p>A register of all hazardous and dangerous goods must be kept on site at all times under the Dangerous Goods & Safety Management Act 2001.</p> <p>The applicant has further advised that in the event of a spill occurring, spill kits will be strategically positioned around the site and fitted to all vehicles to capture any spills immediately at the point source. If a spill of manufactured asphalt occurs, the asphalt sits on the surface of the soil and is easily removed without a risk of contamination of groundwater.</p>	<p>2, 20, 37, 93</p>
<p>What conditions/regulations are currently in place for Boral in regards to dust pollution?</p>	<p>The management of dust is controlled by the works approval issued by the Department of Environment Regulation.</p>	<p>43,</p>
<p>Can Council and the EPA advise me why Boral has not controlled their dust, and will the necessary work be carried out to control dust before any new plant is approved?</p>	<p>The management of dust relating to the existing quarry operation is controlled by the works approval issued by the Department of Environment Regulation. In any event, it is considered that this is a separate matter that is not directly related to the subject application.</p>	<p>72,</p>
<p>3. Traffic and Access</p>		
<p>Concerned about current heavy vehicle traffic, the increases in heavy vehicle traffic as a result of this use, and that the expected vehicle movement figures have been understated.</p>	<p>See comments under the heading Vehicle Movements in the Discussion section of the report.</p>	<p>1, 2, 3, 4, 6, 7, 8, 10, 11, 13, 16, 19, 20, 21, 23, 25, 26, 27, 30, 31, 32, 33, 34, 35, 36, 39, 41, 42, 43, 44, 45, 46, 47, 48, 49, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 65, 66, 67, 70, 71, 72, 73, 74, 78, 79, 80, 82, 83, 84, 85, 88, 89, 90, 91, 92, 93, 95, 96.</p>

Matter Raised	Comment	SR No.
The roads and intersections in the Orange Grove area are not constructed to handle the current or proposed number of expected heavy vehicles.	See comments under the heading Road Infrastructure in the Discussion section of the report.	1, 2, 3, 4, 6, 7, 8, 10, 11, 12, 14, 19, 20, 21, 23, 35, 36, 38, 39, 42, 43, 44, 45, 47, 48, 52, 53, 54, 55, 56, 57, 60, 66, 67, 72, 73, 78, 79, 83, 89, 91, 92, 93, 95.
There are no bike paths or footpaths provided for pedestrians, cyclist or horse riders to use to compensate for the truck traffic. There are deep ditches along Reservoir Road and other roads in Orange Grove making it difficult and unsafe for pedestrians/cyclist/horse riders to walk/cycle/ride along the road when there are truck movements.	See comments under the heading Road Infrastructure in the Discussion section of the report.	1, 2, 3, 4, 6, 7, 8, 9, 10, 11, 12, 13, 14, 19, 20, 21, 23, 27, 30, 32, 34, 36, 39, 42, 43, 44, 45, 47, 49, 53, 54, 55, 56, 57, 59, 60, 66, 71, 72, 73, 78, 79, 82, 84, 88, 89, 90, 92, 93, 95
What Traffic Impact Study has been performed to support this application?	The Western Australian Planning Commission's 'Transport Assessment Guidelines for Developments' provide a guide to assist transport officers in assessing the transport implications of land use development proposals. Based on these guidelines, the proposed development is expected to have a low or moderate impact, and as such a full traffic assessment is not required, rather a brief transport statement should be provided with the development application which is an informal, non-technical statement of the transport aspects of the development. The applicant has provided the necessary information as required by the guidelines.	2, 45, 89, 91.
Are there any plans to upgrade roads and infrastructure to cater for current and future traffic increases?	See comments under the heading Road Infrastructure in the Discussion section of the report.	72, 83, 89
4. Amenity		
Concerned about the impact the use will have the rural amenity and lifestyle of the area.	See comments under the heading Rural Amenity in the Discussion section of the report.	2, 3, 4, 12, 13, 21, 22, 30, 43, 45, 46, 49, 53, 54, 57, 58, 59, 65, 69, 72, 74, 79, 83, 84, 89, 90, 91, 92, 95.
Concerned about current noise levels from the quarry operation and the impact that it is having on surrounding residents by virtue of the hours of operation and/or the operations undertaken/machinery used	The hours of operation and associated noise emissions for the existing quarry operation is controlled by the works approval which is issued, administered and enforced by the Department of Environment Regulation. As such, it is considered that this is a separate matter that is not directly related to the subject application.	1, 4, 10, 19, 21, 24, 26, 29, 53, 54, 72, 73, 78, 84, 89, 93

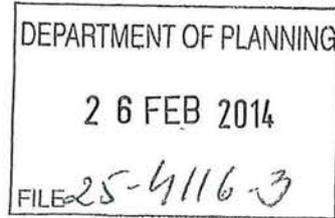
Matter Raised	Comment	SR No.
Concerned about additional noise as a result of the proposed land use and the impact it will have on the amenity of the area.	See comments under the heading Noise in the Discussion section of the report.	2, 3, 6, 10, 11, 12, 21, 24, 26, 37, 38, 43, 46, 52, 59, 71, 73, 80, 83, 91, 92, 95.
Concerned about traffic noise from heavy vehicles and the impact it will have on the amenity of the area.	See comments under the heading Noise in the Discussion section of the report.	4, 8, 14, 23, 30, 43, 48, 65, 73, 84, 96
Concerned about the proposed hours of operation and that the use will occur outside of those hours of operation, and therefore impact on the amenity of the area through adverse noise.	See comments under the heading Hours of Operation in the Discussion section of the report.	2, 10, 26, 37, 39, 51, 55, 56, 57, 65, 70, 71, 83, 88
Concerned about the current and future visual amenity impacts in the form of the 'Darling Scar', emission stacks as a result of the site's operation or views of the site's operation.	See comments under the heading Visual Amenity in the Discussion section of the report.	1, 10, 35, 42, 43, 53, 54, 72, 89, 91, 92.
5. Miscellaneous		
Although the use is proposed to be temporary in nature, concerned that any approval will be a precedent for future asphalt plants to be established at the quarry.	Any similar future applications would be subject to approval by the Western Australian Planning Commission.	1, 5, 10, 23, 32, 37, 43, 47, 53, 54, 56, 57, 73, 83, 88, 90, 91, 92.
Concerned that the plant will stay beyond its removal date of 2016 as it may well be extended for other projects or due to delays.	See comments under heading Term of Operation in the Discussion section of the report.	8, 9, 10, 36, 37, 43, 47, 60, 65
Boral operates an asphalt plant in the industrial area of Welshpool, so why can't the asphalt plant be established there or at the Gateway site itself?	The applicant has advised that it did investigate using a site in Welshpool where there is an existing asphalt plant, however the site was deemed unsuitable due to a lack of available space on the site. In terms of the Gateway site, there are urban areas located within 75m of the site and therefore it was deemed unsuitable as it would be closer to high density sensitive land uses.	1, 2, 4, 10, 23, 36, 53, 54, 59, 66, 73, 91, 92.
Concerned that the blasting shocks and vibration from the quarry are causing deterioration of structures in the area.	The vibration associated with the existing quarry operation is controlled by the works approval which is issued, administered and enforced by the Department of Environment Regulation. As such, it is considered that this is a separate matter that is not directly related to the subject application.	1, 2, 10, 47, 53, 54, 67, 78, 84, 89, 94.
Concerned about the impact that the proposal will have on property values within the area.	This is not a valid planning consideration.	2, 4, 8, 21, 23, 36, 43, 52, 57, 58, 60, 71, 75, 89, 92, 93

Matter Raised	Comment	SR No.
Council's Local Planning Policy means that only residents within 300m of the proposal have been contacted for comment however the existing and anticipated pollution travels much further therefore the wider community should be informed, especially nearby sensitive land uses e.g. schools and aged care facilities.	Council's Local Planning Policy 4.1 - Public Consultation does not technically relate to the advertising of proposals on land reserved for Parks and Recreation. Notwithstanding that, it was considered appropriate to send letters to surrounding landowners within 300m of the subject site, in addition to advertising the proposal on the City's website (including all the technical documentation). As such, concerned residents beyond 300m have had the opportunity to consider the information available and make a submission.	1, 10, 53, 54, 69, 89, 90, 91.
Nothing has been proposed by Boral to engage with the community in relation to ongoing monitoring if the proposal was approved.	The applicant has advised that it undertook its own consultation with its existing Community Reference Group. Furthermore, they provided information packages to local residents which provided information on the proposal. Notwithstanding, this is not a relevant planning consideration as consultation has been undertaken by the City.	66.
The use will impact on community activities in the area i.e. the equestrian centre.	There is no evidence to suggest that the proposed asphalt plant will interfere with existing community activities.	3, 8, 73.
The City has a vested interest in the Boral quarry, with income being received, and as such must make every endeavour to be impartial in their determination of the application to avoid a conflict of interest.	The Western Australian Planning Commission are the determining authority for this application.	60, 91.
Are Council aware or have suggestions for any alternative sites, should this application be unsuccessful?	The applicant has advised that they investigated the feasibility of other sites in the Perth Metropolitan area. Should the subject application be unsuccessful, the City is unaware of what alternative locations the applicant will seek to use for the proposed mobile asphalt plant.	72, 74,

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20 February, 2014

Chairman WA Planning Commission
Locked Bag 2506
PERTH WA 6001



Dear Sir

Boral Proposed Asphalt Plant – Orange Grove

I would like to make you aware that the relevant parties that should be required to consider the complex issues involved in this matter are placing all responsibility onto the WA Planning Commission. I do realize that it is not the responsibility of WA Planning Commission to assess many of these matters, but I am left with very little option but to plea with you to refer this matter to the EPA before making your final decision.

For your consideration:

The proposed site in Orange Grove is zoned for Parks and Recreation not for the noxious industry of asphalt.

The proposed site is apx 3 km from a source of Perth's drinking water "Victoria Reservoir"? A study done by Blue Ridge Environmental Defense League (a well regarded American environmental organization) shows that the emissions of Benzene and Arsenic from an asphalt plant can travel in excess of 3 km. The study is based on 160 tonnes per hour, Boral expected output is 300 tonnes per hour.

The proposed site is in close proximity to schools, a retirement village, private residents, Riding for the Disabled, a pony club, Youth and Christian camps, playgrounds and adjoins Banyowla Regional Park and Korung National Park.

Banyowla Regional Park is potentially the home of various endangered species? Species include Forest Red-tailed Black Cockatoo, Baudins' Cockatoo, Carnaby's Cockatoo, Rainbow Bee-eater, Chuditch and Quenda. This information was obtained directly from Boral's application and it appears that they feel no impact will be made as they are not clearing. Boral have completely ignored the impact of the emissions. It appears that it is universally agreed that exposure to chemicals in asphalt causes cancer and other detrimental effects on animals.

Some of the residents of Orange Grove do not have access to scheme water and presently collect their drinking water from their roofs.

According to the EPA document "Guidance for the Assessment of Environmental Factors" Asphalt Plants should have a buffer distance of 1000m. Some residents and parts of Korung National Park and Banyowla Regional Park fall within the 1 km buffer zone.

Information from the Air Assessment report from Boral's application :

- *"In both modelling approaches however, there are questions as to whether accurate predictions of concentrations at the nearest residences could be made."*
- *"As such with the complexity of the dispersion and doubt whether any modelling could accurately model this, a simplified approach has been selected"*
- *"Predictions were made using the model TAPM for an area to the west of the quarry as TAPM can not model the very steep sided, small pit. The plume rise will be indicative of that which would occur under the normal meteorology."*
- All prediction modelling has been done assuming flat terrain, this is clearly not the case. The site is located 97 m below ground level (a pit). The report states that the plume height will be in the range of 70 to 150 m above ground level. *"At these heights, the plume would be near the top or above the west quarry wall..."* (I think that means ground level of Banyowla Regional Park???)
- Meteorological data has been used from Red Hill a site located 20 km from Orange Grove and not of the same topography.
- The report contains very little information in regards to the toxic emissions.
- Metals have not been considered.
- Animal health has not been considered.

I'm certainly not an authority on such matters but I would consider the OEPA would be well placed to carry out the required studies to determine if the conclusions within this Air Assessments report are reasonable? To the layman it appears that the Air Assessments report is not reasonable, but it is not my place to assess such matters, it is the job of the EPA.

The EPA considers the matter insignificant based on the advice of the DER? I am currently waiting on that advice to substantiate on what basis the DER considers the operation of an Asphalt Plant is deemed to be insignificant through the FOI process.

I questioned an employee (name withheld) at the DER as to why the EPA did not consider this a Significant Proposal, her reply implied that the DER cannot control what the EPA considers to be significant, yet a document written by Anthony Sutton (EPA Assessment and Compliance Division) to Boral reads "Based on the information supplied by Boral and the advice from the DER, the OEPA considers the proposed mobile asphalt plant can be appropriately managed under the existing conditions and commitments set out in Statement 170, as well as through amendments made to your Licence conditions which are being reviewed by the DER." Please note that Statement 170 refers to expansion of quarry activities, specifically the quarry itself, there is no reference to the operation of an Asphalt Plant.

Asphalt fumes are known toxins.

Boral won the Gateway WA project based on using the current correctly zoned Welshpool plant.

These are just some of the issues.

When making your decision, please consider the following choices

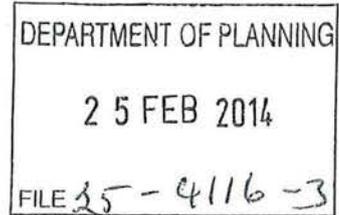
1. Allow Boral to make loads more money and potentially impact the community's health and environment.
2. Say no to Boral and make them use the Welshpool site, which is currently zoned for this activity.

Seems like an easy choice to me.

The Orange Grove quarry is clearly not an appropriate site to operate a temporary asphalt plant.

Please refer this proposal to the EPA.

Thank you for your consideration.



24 February, 2014

Chairman WA Planning Commission
Locked Bag 2506
PERTH WA 6001

Dear Sir

Boral Proposed Asphalt Plant – Orange Grove

I am deeply concerned that the DER and EPA have made an oversight when assessing the Boral application. I am left with little option but to plead with you to refer this matter to the EPA before making your final decision.

Please consider the following information. Statements made in italics are taken from the Air Assessments Report.

Emission Estimates

The Baghouse Exhaust stack emission estimates of VOCs, Benzene, PAHs (BaP) TEQ, SO₂ and odour from the baghouse exhaust stack have been taken from the Welshpool Plant. No data was available for the temporary plant.

When the mobile asphalt plant was tested in Redbank by GHD 2010, why didn't they conduct emission estimates?

Metals have not been considered.

The stack height has been increased to 15.5 m and a reduced diameter to .6 m.

Why?

Load out of Trucks emissions include VOCs(including odour),CO and PM.Additionally for some plants and conditions, blue smoke, primarily condensed VOCs is visible (Sutton 2002 and Fulton Hogan 2013). No local WA emission data was available.

Again GHD 2010 Victoria data was used and it only tested for odour, what about the toxic emissions?.

It was noted in the GHD (2010) report that this was conducted using Shell bitumen and that odour emissions would be dependent somewhat on the bitumen used. Therefore odour emissions are indicative only and may vary with bitumen source and also by load out time.

Estimated Existing Concentrations in the Area

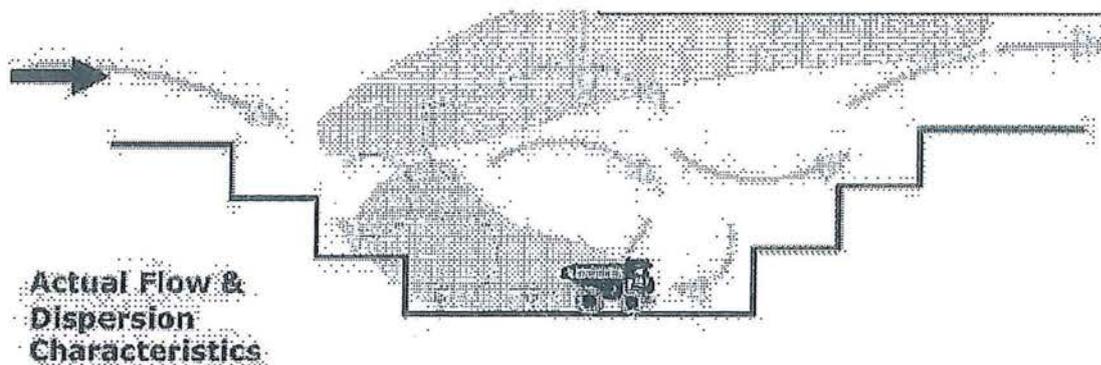
Anticipated existing concentrations for the Orange Grove area are based on concentrations at Caversham (where available), then South Lakes and Hope Valley.

These existing concentrations are taken from sites approximately 20 km from Orange Grove

Modelling Methodology

The terrain at the site is a very steep sided quarry, with a wall of approximately 70 to 90m above the quarry floor. The quarry is essentially enclosed on all sides, except for an opening to the south where vehicles access.

The image below taken from the Boral Application - Air Assessments Report (Figure 5-2) It is incorrectly scaled. If the pit is 97m metres deep then the truck depicted in the figure is 23 metres high!!!!



There is the potential for some build up of concentrations in the pit as the base of the pit is about 20m lower than the entrance.

Boral employees will be working in this!!

The plume height will be in the range of 70 to 150 m above ground level. At these heights, the plume would be near the top or above the west quarry wall

This means ground level!!!!

There is little likelihood that the plume would end up near the quarry floor, instead the plume will be towards the top of the pit and therefore have a greater likelihood of being vented out of the pit.

What about the sensitive users at the top of the pit within the 1000m buffer zone? Please refer to my layman's diagram attached Diagram 1 and Diagram 2

Model Selection

In both modelling approaches however, there are questions as to whether accurate predictions of concentrations at the nearest residences could be made.

Predictions were made using the model TAPM for an area to the west of the quarry as TAPM can not model the very steep sided, small pit. The plume rise will be indicative of that which would occur under the normal meteorology.

As such with the complexity of the dispersion and doubt whether any modelling could accurately model this, a simplified approach has been selected.

For this screening, the model AUSPLUME has been selected where all terrain is neglected such that straight line dispersion is modeled.

Clearly not the case.

Air Assessments Report bases its modelling on flat terrain

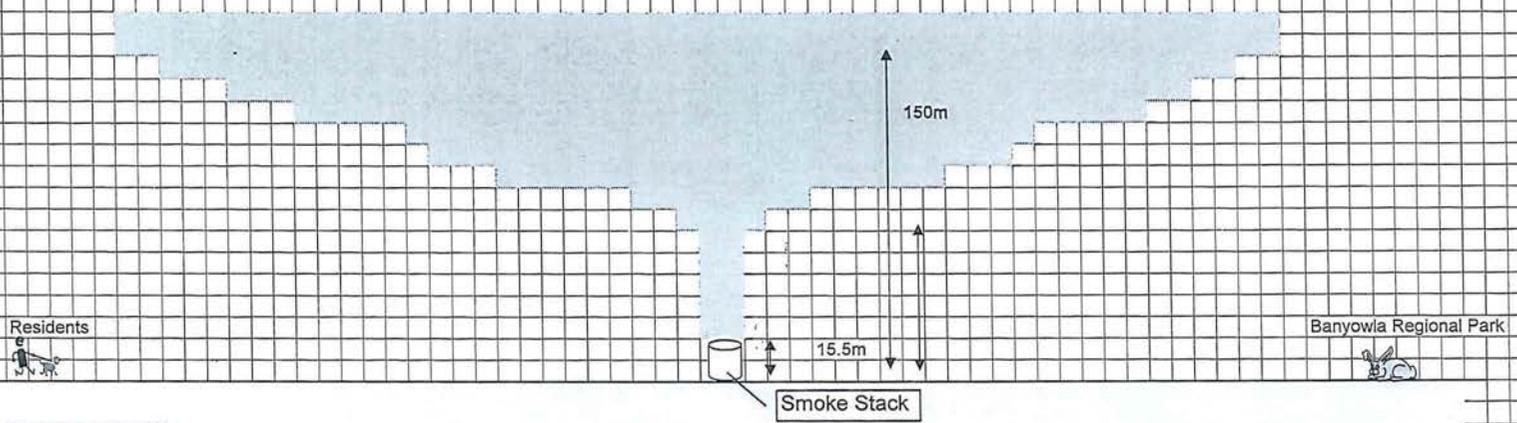


Diagram 1

Actual terrain

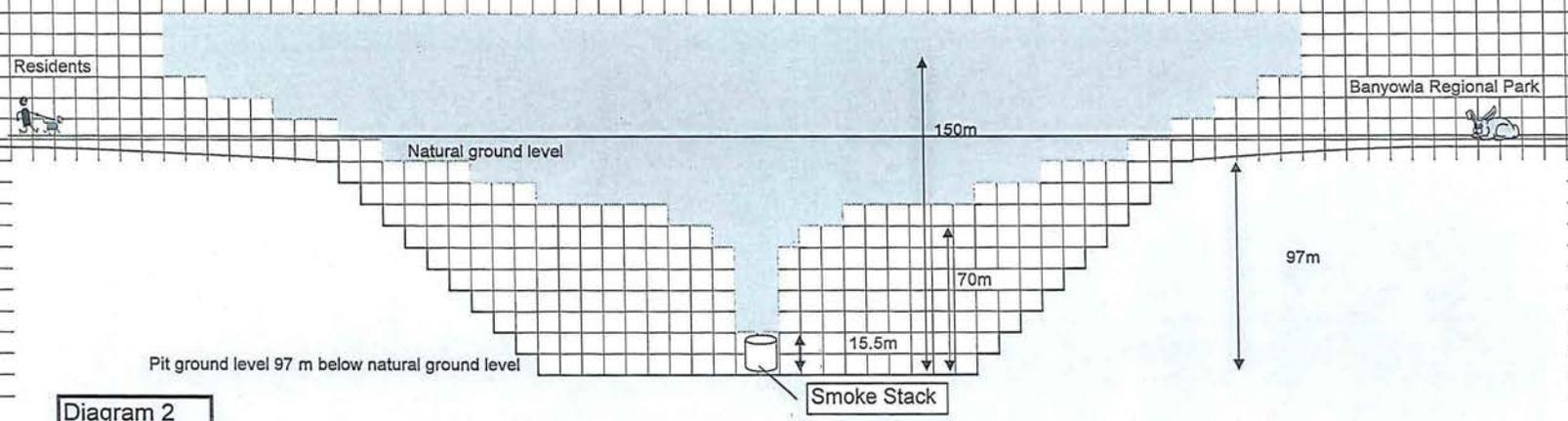


Diagram 2

It is only considered that higher concentrations to the residences to the west could occur, if under stable conditions, ponding occurred in the quarry and if this was vented out in the early morning when the stability within the quarry started to breakdown causing flushing to occur.

What about when the plume reaches the top of the pit (ground level) and the easterly wind blows in the direction of the residents?

I'm certainly not an authority on such matters but I would consider the OEPA would be well placed to carry out the required studies to determine if the conclusions within this Air Assessments report are reasonable? To the layman it appears that the Air Assessments report is not reasonable, but it is not my place to assess such matters, it is the job of the EPA.

It would seem reasonable that if an asphalt plant was placed in a 97m deep pit, the plume must be vertical, it cannot be dispersed other than through the small vehicle opening or to go straight up. The plume height is expected in the range of 70 – 150 m above ground level. So the real ground level is where the plume starts to disperse. The plume should be dispersing at 70-150 m above ground level not at ground level. Seems like rather large oversight to me. I did contact Owen Potts who prepared the report but he declined to speak with me.

The EPA, to this point considers the matter insignificant based on the advice of the DER? Please ask them to reconsider .

Asphalt fumes are known toxins.

Please refer this proposal to the EPA.

Thank you for your consideration.